



City of
NELSON
BRITISH COLUMBIA

Downtown Urban Design Strategy



DRAFT VERSION 5
APRIL 29, 2017

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In association with
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TABLE OF CONTENTS

ACKNOWLEDGEMENTS

EXECUTIVE SUMMARY	i
-------------------	---

1 INTRODUCTION	1
----------------	---

1.1 Study Area	2
----------------	---

1.2 Planning Framework and Objectives	3
---------------------------------------	---

1.3 Downtown Nelson	5
---------------------	---

1.3.1 Unique Characteristics	5
------------------------------	---

1.4 Planning Process	6
----------------------	---

1.5 Public and Stakeholder Consultation	7
---	---

2 FRAMEWORK FOR URBAN DESIGN	11
------------------------------	----

2.1 Street Structure and Defining Characteristics	12
---	----

2.2 Unifying and Distinct Elements	16
------------------------------------	----

2.3 Pedestrian Priority	21
-------------------------	----

2.4 Respect for Heritage	22
--------------------------	----

2.5 Vibrancy Through Streetscape Design	24
---	----

2.6 A Multi-Modal Transportation System	25
---	----

2.7 Accessibility and Safety	29
------------------------------	----

2.8 Public Amenity Space for All to Use	30
---	----

2.9 Urban Landscape Ecology	31
-----------------------------	----

2.10 Arts and Culture	32
-----------------------	----

2.11 Wayfinding and Signage	33
-----------------------------	----

3 RECOMMENDATIONS FOR URBAN DESIGN	34
------------------------------------	----

3.1 Design Principles	35
-----------------------	----

3.2 10 Big Moves	36
------------------	----

3.3 Design Recommendations	38
----------------------------	----

3.3.1 Sidewalk Definition and Building Edge	38
---	----

3.3.1.1 Sidewalk Zones	38
------------------------	----

3.3.1.2 Building Edge	40
3.3.2 Sidewalks, Streets, Intersections and Lanes	41
3.3.2.1 Sidewalks	41
3.3.2.2 Streets, Parking and Intersections	43
3.3.2.3 Use of Streets for Special Events	52
3.3.2.4 Parking Opportunities in the Downtown	55
3.3.2.5 Lanes	57
3.3.2.6 Bicycle Support in the Downtown	59
3.3.2.7 Transit Station	60
3.3.3 Public Amenity Spaces	63
3.3.3.1 Mid-Block Amenity Areas	63
3.3.3.2 Bulb-out Amenity Spaces	65
3.3.4 Outdoor Patios	67
3.3.5 Awnings and Canopies	72
3.3.6 Signage	74
3.3.6.1 Specific Signage Types	76
3.3.6.2 Wayfinding	79
3.3.7 Lighting	83
3.3.7.1 Street Lighting	84
3.3.7.2 Holiday Lighting	86
3.3.7.3 Building Lighting	87
3.3.8 Street Furniture	88
3.3.8.1 Benches	89
3.3.8.2 Waste/Recycling Bins	91
3.3.8.3 Bike Racks	93
3.3.8.4 Water Fountains	95
3.3.8.5 Bollards	96
3.3.8.6 Paving	98
3.3.9 Landscaping	100
3.3.9.1 Urban Trees	100
3.3.9.2 Planting Beds	104
3.3.9.3 Container Plantings and Hanging Baskets	105
3.4.1 Public Art	106

3.4.1.1 Sidewalks	107
3.4.1.2 Murals	108
3.4.1.3 Utility Boxes	110
3.4.2 Washrooms	111
3.4.3 Electric Vehicle and Car Share Parking	113
4 IMPLEMENTATION	114
4.1 Introduction	114
4.2 26 Principal Actions	117
4.3 Implementation Strategy	120
4.4 High Level Costing	123
4.5 Construction Method	128

EXECUTIVE SUMMARY



EXECUTIVE SUMMARY

OVERVIEW AND PURPOSE

The Downtown Urban Design Strategy is a framework to guide public realm improvements in Downtown Nelson. Collectively, the public spaces in the downtown referred to as the public realm, are the outdoor ‘living room’ of the city that includes the streets, sidewalks, lanes and amenity spaces on Baker, Vernon, and Victoria Streets, as well as all of the cross streets between Highway 3A and Cedar Street.

The downtown is the commercial and cultural heart of Nelson and the City is committed to ensuring that it continues to be a vibrant and progressive place in the future. This strategy provides a comprehensive vision for the downtown public realm, as well as a series of recommendations to achieve the vision and to address Council priorities for sidewalk patios, signage and lighting. The recommendations also provide direction for public realm improvements that may be completed when aging above- and below-ground infrastructure is replaced in the near future.

This strategy was developed through careful analysis of the downtown and it is built on significant input from stakeholders and the public. It builds on the vision of the *Sustainable Waterfront and Downtown Master Plan (SWDMP)*, as well as the direction articulated in a number of City policies and guidance documents. It includes the following sections:

Section 1 describes the planning framework and articulates the unique characteristics of Nelson’s downtown. It also outlines the planning process and describes the steps that were taken to engage stakeholders and the public.

Section 2 outlines the framework for urban design, which builds on the structure and unique characteristics of the individual streets in the downtown, and describes the unifying and distinct elements for urban design.

Section 3 details the urban design strategies for the downtown public realm including strategies for sidewalks, amenity spaces, outdoor patios, awnings and canopies, signage, street furniture, landscaping, public art, washrooms, and electric vehicle and car share parking.

Section 4 focuses on implementation and provides detailed information on costing, as well strategic direction on timing, partnerships and funding, and construction methods.

VISION

Nelson’s downtown is the vibrant and engaging heart of the community. Residents and visitors enjoy the unique local shops, the rich history and the diverse cultural activities. It is easier to walk and cycle in the downtown with improved sidewalks and shared bike and car travel lanes that are supported by easy wayfinding and bike facilities. A coordinated and flexible streetscape design provides for a creative, safe and accessible downtown that has frequent festivals and celebrations in the new “town square” at the intersection of Ward and Baker Streets. Other activities in the downtown continue to expand into the evening and across four seasons with improved lighting and convenient washrooms, as well as more available parking.

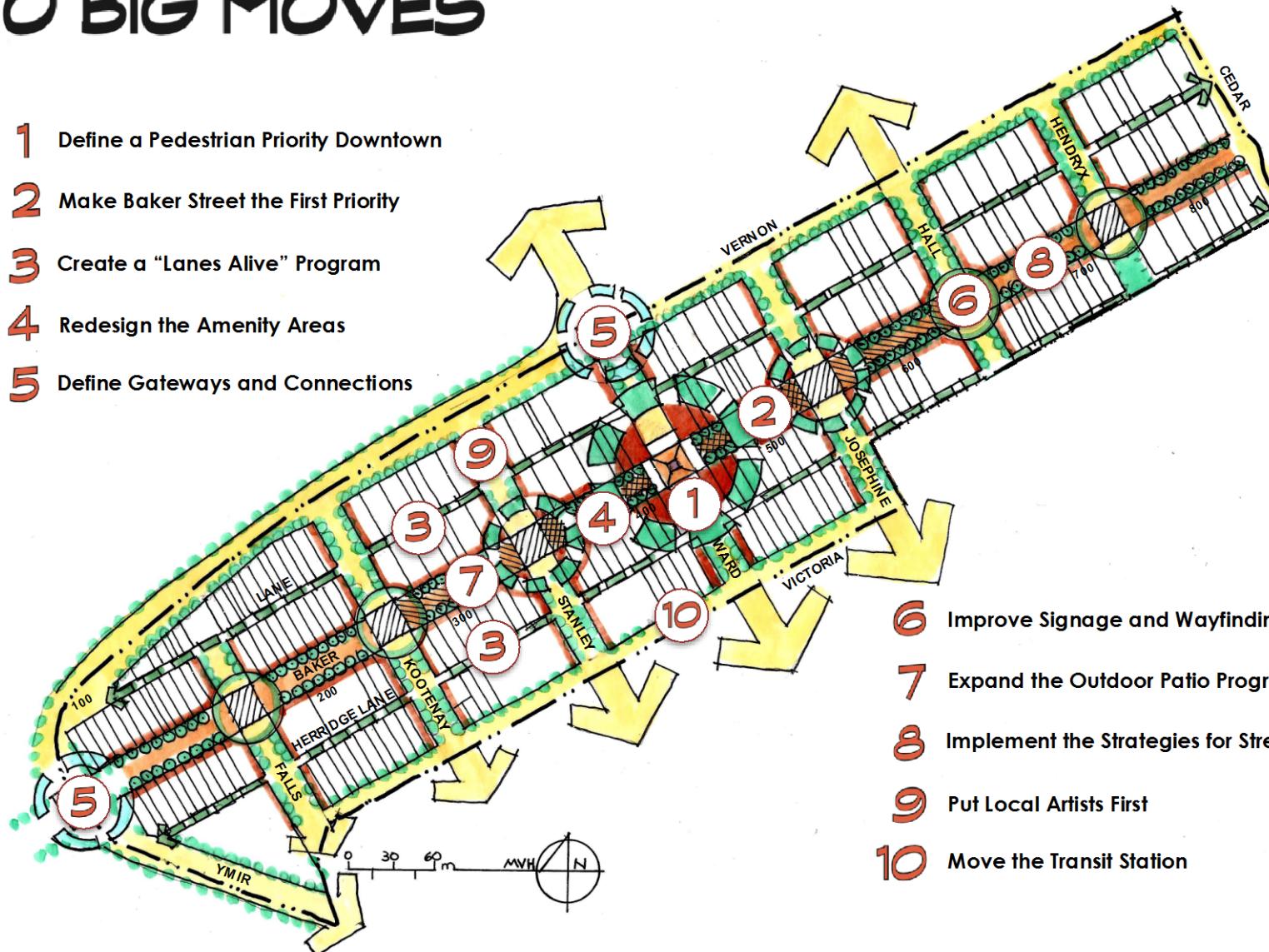
There are increased opportunities to meet and linger in the public amenity spaces, the outdoor patios, or in one of the many bulb-outs that have a variety of seating options, public art and plantings.

Nelson’s downtown has retained its authentic character, while at the same time expanded its potential as an active, resilient and engaged business, arts and culture community. It has found a dynamic balance between heritage conservation and progressive vitalization.



10 BIG MOVES

- 1 Define a Pedestrian Priority Downtown
- 2 Make Baker Street the First Priority
- 3 Create a "Lanes Alive" Program
- 4 Redesign the Amenity Areas
- 5 Define Gateways and Connections



- 6 Improve Signage and Wayfinding
- 7 Expand the Outdoor Patio Program
- 8 Implement the Strategies for Street Furniture
- 9 Put Local Artists First
- 10 Move the Transit Station



10 BIG MOVES

There are 10 big moves that will advance the improvement of Nelson's downtown:

1 Define a Pedestrian Priority Downtown

Ensure that pedestrians come first in planning and design of the streets and lanes through a flex-design that is adaptable to pedestrian, cyclist and vehicle depending on the activities

2 Make Baker Street the First Priority

Design a "Town Square" and four-block area at the intersection of Baker and Ward Streets. Introduce bulb-outs at street intersections, and redesign the existing public amenity areas

3 Create a "Lanes Alive" Program

Create pedestrian and bike lanes or "thru-ways" through the downtown

4 Redesign the Amenity Areas

Redesign Baker Street amenity areas and create new bulb-outs to increase public amenity areas in the downtown

5 Define Gateways and Connections

Improve the downtown entrances and intersections at the Railtown District and City Hall intersections with painted crosswalks, gateway signage and landmarks

6 Improve Signage and Wayfinding

Amend the sign bylaw and eliminate sandwich boards in favour of landmark directional signs at street corners to improve safety and wayfinding

7 Expand the Outdoor Patio Program

Amend the existing regulations to improve the look and performance of existing outdoor patios

8 Implement the Strategies for Street Furniture

Coordinate a street furniture program for each of Baker Street, Vernon Street, Victoria Street and the other connector streets to create unity, yet still provide distinct elements like public art, murals, and planters

9 Put Local Artists First

Create an overall public art placement strategy, as well as partnerships with local artists and fabricators coordinated with the proposed bulb-outs and laneway improvements

10 Move the Transit Station

Consider the transit station relocation from Ward and Baker Streets to Victoria Street



IMPLEMENTATION SUMMARY

Implementation will involve physical changes to the downtown, such as street reconstruction corresponding to infrastructure upgrade projects. It will also involve changes to existing policy or regulatory documents, and the addition of incentive programs like "Adopt-A-Block". These priorities for implementation are organized into short, medium, and medium to long-term timeframes that will be acted upon as time and resources allow.

The implementation plan is summarized in the table below. A more detailed explanation including more exact timing, resources, responsibilities and construction approach is outlined in Section 4.

TABLE ABBREVIATIONS

NDCC	Nelson & District Chamber of Commerce
NAEDP	Nelson & Area Economic Development Partnership
CDC	Cultural Development Committee
NDAC	Nelson & District Arts Council

PHASE AND COMPONENT		DESCRIPTION	PROJECT LEAD	KEY PARTNERS	FUNDING	COST ¹
Short Term Priorities (1-2 years)						
1	Baker Street Stage 1	Reconstruct the 100, 200 and 300 blocks of Baker Street using the intermediate treatment for street improvements.	City	BC Transit, NDCC, CDC	Capital budget and provincial/federal grants	3 blocks x \$840,000/block = Est. \$2.52 million (intermediate treatment). A bike shelter in the 200 block is an additional \$30,000 to \$50,000.
2	Street Light Replacement	Replace street lights in two phases: Phase 1 – (Baker Street) 100 lights Phase 2 – (remainder of downtown) 232 lights, depending on condition. The recommendation is to increase lighting in the 400 and 500 blocks of Baker Street (east and west) pending verification from a lighting specialist.	City	MOTI	Capital budget	\$10,000 per light Phase 1 = Est. \$1.0 million Phase 2 = Est. \$2.32 million
3	Policy, Regulation & Guideline Amendments	Amend policy, regulatory and guideline documents to incorporate recommendations for signage, awnings and canopies, and outdoor patios.	City	NDCC, NAEDP, Land Owners	Development Services work program	Allocate \$25,000 consulting budget for rewriting policy and regulations
4	Gateway signage + Wayfinding	<ul style="list-style-type: none"> • Gateway sign at Baker Street at Hwy 3A entrance; • Gateway sign at Ward and Vernon Streets intersection; and • Secondary wayfinding markers at street corners; and Digital sign pilot project. 	City	NDCC, NDAC, CDC NDCC (digital sign)	Capital budget NDCC (digital sign)	Baker Street at Hwy 3A: \$25,000 – \$50,000 Ward and Vernon Street intersection: \$25,000 - \$35,000 Secondary wayfinding markers: \$10,000 - \$20,000 each Digital Sign: Price TBD



Phase and Component		Description	Project Lead	Key Partners	Funding	Cost ¹
Short Term Priorities (1-2 years)						
5	Baker Street + Hwy 3A Intersection Improvements (refer to Railtown SNAP for more information)	Work with MOTI to increase pedestrian safety including painted crosswalks, removal of the right turn option, construction of corner bulb-outs, exploring the use of streetlights/round-about, and painting of parking stalls on the bridge.	City	MOTI, NDCC, local businesses	Capital budget	Crosswalk painting \$1,500; 4 corner bulb-outs \$65,000; remove right turn option \$15,000; install street lights \$150,000 (refer to Railtown SNAP). Est. \$231,500
6	Ward and Vernon Streets Intersection + Hwy 3A Intersection Improvements	Work with MOTI to increase pedestrian safety at intersections including painted crosswalks and construction of 2 corner bulb-outs, if right turn movements permit.	City	MOTI, NDCC, local businesses	Capital budget	Crosswalk painting: \$1,500; 2 corner bulb-outs: \$40,000 ² = est. \$41,500
7	Lanes Alive Program Stage 1	Improvements to the aesthetics and function of the lanes. Adding pedestrian and bike infrastructure. Focus upgrades to 400 and 500 blocks of the lanes, between Josephine and Stanley Streets.	City	CDC, NDAC, adjacent owners and businesses	Capital budget and local businesses	\$100,000 for lighting and painting x 2 blocks x 2 lanes = \$400,000 (est.)
8	Baker Street Adopt-A-Block Incentives	Implement a matching grants program to incentivize façade improvements including building lighting, signage, awnings and canopies, and outdoor patios.	City	NDCC, NAEDP, adjacent owners and businesses	Capital budget and provincial/federal grants	Recommended pilot program: potential 10 matching grants first year of \$5,000 to \$10,000 for a total \$50,000 to \$100,000 per year; total of \$150,000 to \$300,000 committed over 3 years.
Medium Term Priorities (3-5 years)						
9	Baker Street Stage 2	Reconstruct the 400 and 500 blocks of Baker Street, as well as Ward Street between Victoria and Vernon Streets using the premium treatment. The reconstruction includes a redesign of the public amenity areas, as well as the addition of new street and sidewalk treatments, catenary lighting at the intersection of Ward and Baker Streets, a community table and overhead trellis, street furnishings, and a water fountain.	City		Capital budget and provincial/federal grants	\$1.1 million per block x 4 blocks = est. \$4.4 million (premium treatment).
10	Victoria Street Stage 1	Redesign the 200 and 300 block to a one way road (eastbound) using the standard treatment. Convert existing parallel parking to angle parking, where possible. Redesign the 400 block using the standard treatment as well.	City	BC Transit	Capital budget and provincial/federal grants	\$660,000 per block x 3 blocks = est. \$1.98 million (standard treatment).
11	Relocate Bus Transit Stop to Victoria Street	300 block (or to be determined) and facilities including bus shelter, bike shelter, public washroom and water fountain	City	BC Transit, adjacent land owners and businesses	Capital budget and provincial/federal grants	Bus transit stop = \$250,000 to \$350,000 Bus shelter = \$100,000 to \$150,000 Bike shelter = \$30,000 to \$50,000 Washroom = \$100,000 to \$130,000 Water fountain = \$6,000
12	Lanes Alive Program Stage 2	Improvements to the aesthetics and function of the lanes. Focus upgrades in the 300 and 600 blocks of the lanes, between Hall and Kootenay Streets.	City, CDC	NDAC, adjacent landowners and businesses	Capital budget and businesses	\$100,000 for lighting and painting x 2 blocks x 2 lanes = est. \$400,000



PHASE AND COMPONENT		DESCRIPTION	PROJECT LEAD	KEY PARTNERS	FUNDING	COST ¹
Long Term Priorities (6-15 years)						
13	Baker Street Stage 3	Reconstruct the 600 block using the intermediate treatment and the 700 and 800 blocks using the standard treatment.	City		Capital budget and provincial/federal grants	1 block x \$840,000 = est. \$840,000 (intermediate treatment) 2 blocks x \$660,000 = \$1.32 million (standard treatment) = est. \$2.16 million additional bike shelter cost in 700 block of \$30,000 to \$50,000
14	Victoria Street Stage 2	Reconstruct the 500 block using the standard treatment.	City		Capital budget and provincial/federal grants	Est. \$660,000 (standard treatment)
15	Vernon Street Stage 1	Reconstruct the 500 and 600 block (Ward Street to Hall Street), including sidewalks, median and parking. ²	City	MOTI	Capital budget and provincial/federal grants	2 blocks x \$840,000 = est. \$1.68 million (intermediate treatment) ³
16	Vernon Street Stage 2	Reconstruct the 700 and 800 blocks (Hall Street to Cedar Street), including sidewalks, median and parking.	City		Capital budget and provincial/federal grants	2 blocks x \$660,000 = est. \$1.32 million (standard treatment)
17	Lanes Alive Program Stage 3	Complete remaining blocks including the 100 and 200 blocks, and the 700 and 800 blocks in that order. Note: there is no lane in the 100 block of Herridge Lane.	City, CDC	NDAC, adjacent landowners and businesses	Capital budget and businesses	\$100,000 per block x 7 blocks = est. \$700,000
18	Other connector streets	Reconstruct the north/south connector streets starting at Ward Street and working east to west: Josephine Street (2 blocks, standard treatment) Stanley Street (2 blocks, standard treatment) Kootenay Street (2 blocks, standard treatment) Hall Street (already complete) Falls Street (1.5 blocks, standard treatment) Hendryx Street (1block, standard treatment) Cedar Street (1 block, standard treatment)	City		Capital budget and provincial/federal grants	Remaining streets \$660,000 x 9.5 blocks = \$6.08 million (standard treatment)

Notes:

1. See section 4.4 for details on high level costing, including a description of “premium”, “intermediate”, and “standard” treatments.

2. Bulb-out costs will vary depending on design and treatment.

3. Vernon Street Stage 1 – Assume that MOTI is responsible for improvements of the 100 to 400 blocks of Vernon Street, as it is a Provincial highway.



INTRODUCTION



1

1 INTRODUCTION

Nelson is in the heart of the Kootenays and the downtown has a long and rich history that remains present in its streets today. The original downtown underwent reconstruction in the early 1980's.

To address a number of Council priorities and in anticipation of the need to upgrade and replace aging sub-surface and surface infrastructure, this Downtown Urban Design Strategy lays the foundation for the next generation of public realm renewal.

Feedback from stakeholders and residents indicates a preference for a more pedestrianized downtown with coordinated signage, improvements to wayfinding, more and better lighting, organized sidewalks, more public amenity areas, and improved back lanes for pedestrians and cyclists.

This following section provides an overview of the planning framework and objectives for this project as well as the unique characteristics of the downtown, and the public engagement processes.

"Don't spoil the Downtown – we like its character and uniqueness" – Nelson Resident



1.1 STUDY AREA

The sixteen-block area in downtown Nelson is bounded by Highway 3A in the west and Cedar Street in the east, and it includes Baker, Vernon, and Victoria Streets – the three main commercial streets in Nelson.

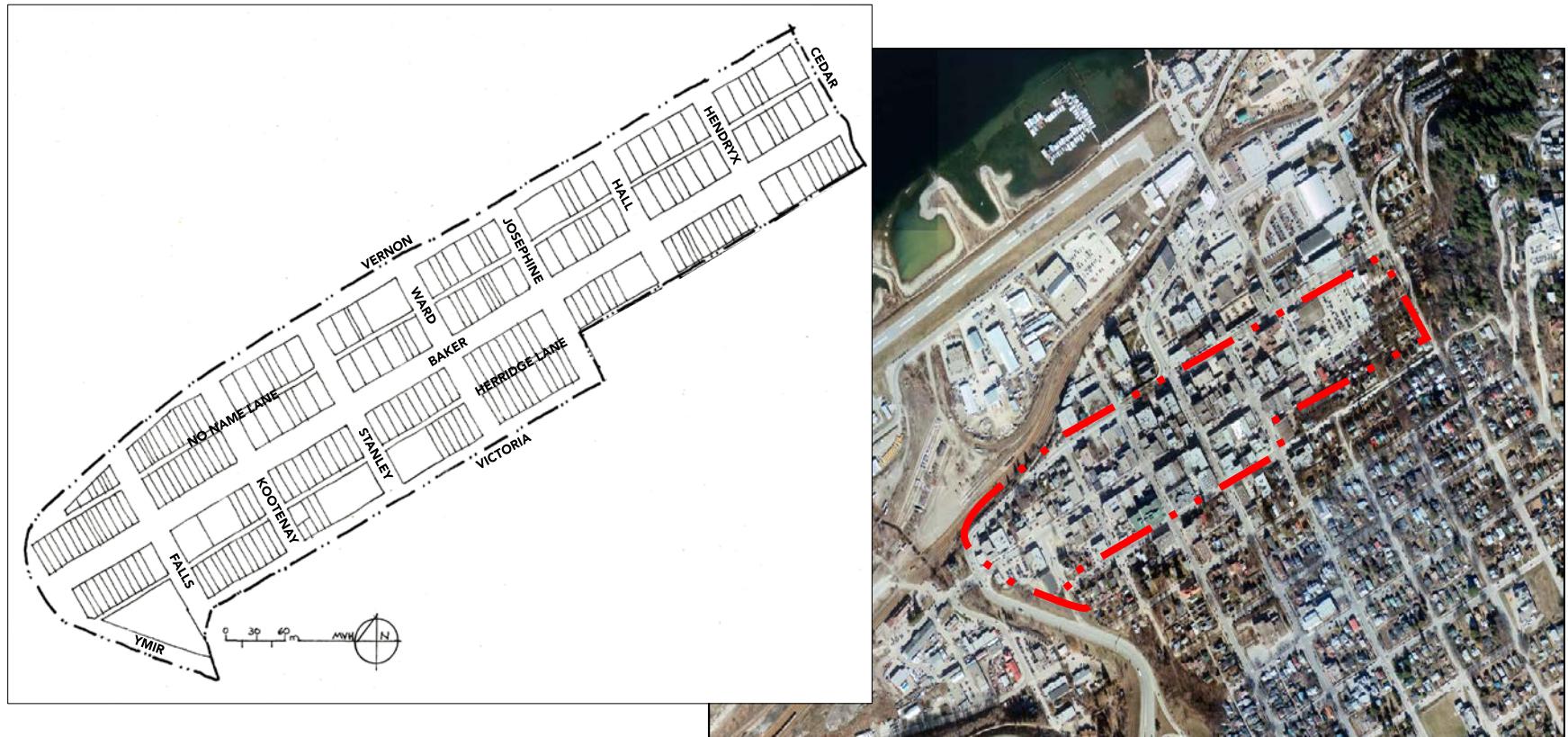


Figure 1: Context map of the Downtown Urban Design Strategy area



1.2 PLANNING FRAMEWORK & OBJECTIVES

Recommended strategies for urban design in Downtown Nelson build on previous reports and studies commissioned by the City. Considerable thought has been given to how the downtown will develop in the future. Nelson's past evolution, current commercial and cultural mix, as well as future opportunities for creative infill in the lanes and along its edges reinforces the downtown's central role as the commercial, cultural, social, institutional and service hub for residents throughout the region.

The City's *Official Community Plan* acknowledges that there a variety of evolving styles within the downtown rather than 'theme' designs based on a fixed style or period.

The City's commitment to sustainable development is highlighted in the *Sustainable Waterfront & Downtown Master Plan*. It includes water and energy conservation, a strong focus on walking within the Downtown core and a desire to provide transportation choices including cycling, public transit and electric cars as well as a variety of housing options close to the core.

Key components of the *Sustainable Waterfront and Downtown Master Plan* that apply to the downtown include:

- Creating additional pedestrian and vehicular linkages between downtown and the waterfront.
- Creating gateways and gathering places.
- Maintaining and enhancing primary neighbourhood streets in the downtown, including Baker Street, Hall Street and Ward Street.
- Preserving view sight lines from the downtown to the waterfront.
- Restricting further intensification of restaurant and commercial uses fronting lanes until the primary commercial streets (Baker, Vernon, Hall and Victoria Streets) are more fully occupied with commercial and other active uses.
- Reconfiguring street parking to add more angle stalls.
- Enhancing mobility in the downtown by designating Baker Street as a shared route for cars and bicycles.
- Incorporating public art where possible in the downtown.



The eight design principles (see Section 3.1) guide the Downtown Urban Design Strategy and build on the City's current objectives, policies and guidelines, as well as recommendations presented in various studies and reports completed over the past decade. The following documents, among others, inform many aspects of the public realm design that are addressed in this report:

- City of Nelson Official Community Plan, Bylaw 3247, 2013 (including Schedule H – Development Permit Area Design Guidelines)
- Nelson Path to 2040: Sustainability Strategy, 2010
- Sustainable Waterfront & Downtown Master Plan, 2011
- Active Transportation Plan, 2010
- Nelson Affordable Housing Strategy Update, 2014
- Low Carbon Path to 2040 – Community Energy and Emissions Action Plan, 2011
- Community Heritage Register Update, 2011
- Tree Management Plan, 2012
- Sign Regulation Bylaw 2027, 1983
- Sign Permits, Sandwich Boards Policy, 2012
- Art in Public Places Policy, 2014
- Comprehensive Cultural Policy, 2013
- Murals, Development Permit Guidelines Policy, 2012
- Outdoor Cafés & Use of Sidewalk & Parking Spaces Policy, 2012
- Bicycle Parking Design Guidelines, 2013
- Nelson and Area Business Retention and Expansion Final Report, 2010
- Roger Brooks - Visitor/Tourism Assessment, 2006

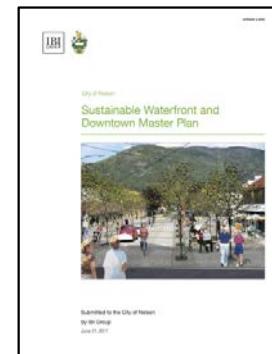


Figure 2: Documents reviewed for the Downtown Urban Design Strategy



1.3 DOWNTOWN NELSON

1.3.1 Unique Characteristics

Nelson has a unique downtown contained by surrounding sloped topography. It is relatively flat and accessible due to the traditional small block grid street system. It is easy to walk and takes approximately five minutes to get from the centre to the edge. The downtown is also visually permeable and connected to the scenic lake and mountain setting. Finally, the rich collection of historic buildings and intact streetscapes create a robust and interesting downtown. The following illustration further describes and diagrams these qualities:

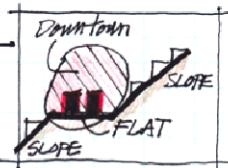
CONTAINED

- Defined Boundaries
- Distinctive Character
- Strong Definition



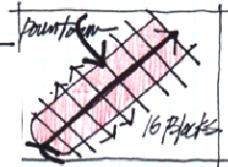
FLAT

- Contrast to Sloped Area
- Easy to Move
- Limited Area



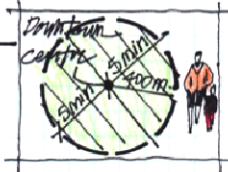
ACCESSIBLE

- Distinct Gateways
- Street Grid Allows Access/Egress
- Familiar Pattern



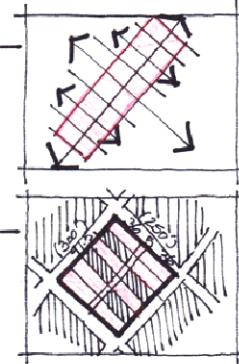
WALKABLE

- Five Minute Walk from Centre
- Ten Minute Walk Downtown
- Easy to Walk



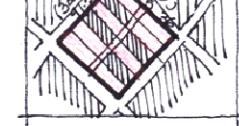
PERMEABLE

- Multiple Access Points
- Alternative Routes
- Visual Orientation



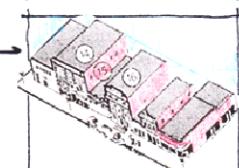
PEDESTRIAN

- Smaller Blocks
- Lane Access
- Small Parcels/Storefronts



DIVERSE

- Variety of Uses
- Local Signatures
- Always Changing



RICH

- Well-Executed Heritage Conservation
- Colour and Texture Variation
- Block to Block Uniqueness



MEMORABLE

- Collective Distinctness
- Unique Experience
- Timeless Architecture

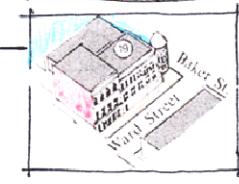


Figure 3 – Illustration of Nelson's Characteristics

1.4 PLANNING PROCESS

The consultant team worked closely with City staff to learn about current challenges and opportunities related to urban design in the downtown area. Consultations with the Nelson & District Chamber of Commerce, business leaders, property owners, tenants, community organizations, including the Nelson Street Culture Collaborative, and citizens were an integral part of this process. Each meeting provided new insights into valued aspects of the public realm.

The planning process involved four phases:

Phase 1 - Spring 2016: Completed an inventory of the public realm and hosted two public meetings, as well as informal conversations with business owners and community leaders. Information gathered in Phase 1 was compiled in a Downtown Urban Design Strategy Progress Report in June 2016.

Phase 2 - Summer 2016: Hosted a three-day design charrette that included the Design Fest workshop and a public presentation of ideas and concept sketches. Those in attendance were asked to complete a questionnaire. Pop-up displays were also held throughout July and August in conjunction with the Wednesday markets on Baker Street.

Phase 3 – Fall 2016: Preparation of the final draft Downtown Urban Design Strategy.

Phase 4 – Winter/Spring 2017: Final Open House and presentation to City Council

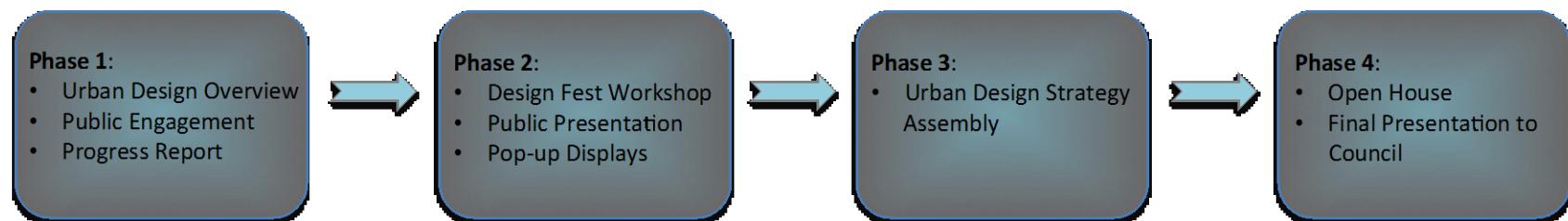


Figure 4: Planning Process



1.5 PUBLIC AND STAKEHOLDER CONSULTATION

Throughout the planning process, the consultant team was inspired by community members, business owners, staff and civic leaders who shared their vision for Downtown Nelson. In addition to the community workshops and public presentation, citizens were invited to share their views at pop-up displays throughout the summer, and by completing the Downtown Health Action Tool Assessment, a questionnaire, and comment sheets at events.

The Urban Design Strategy website (www.nelsonurbanbandesign.com) was an integral part of the communications strategy with background information about the project, as well as updates on public events and displays. The website was also used to solicit feedback through a questionnaire and a short survey.

Other communication tools that were used to help promote this project included Facebook, newspaper ads in the *Nelson Star*, email, and hand-delivered letters to 400 businesses and property owners at the start of the project.

The following is a summary of the community engagement process.

Event #1 - May 16, 2016: "Working Group Meeting with the Business Community and Key Stakeholders"

Guiding Questions:

- What physical design improvements and programs will help downtown Nelson (e.g. lighting, seating, signage, sidewalk patios, extended business hours, more special events)?
- What are the main physical design challenges in downtown Nelson?
- How can the business community and key stakeholders contribute to improvements in the downtown?

Highlights from Participants' Comments:

- Signage: consistent enforcement (e.g. sandwich boards).
- Wayfinding - helping people to plan their visit downtown.
- Street culture is a problem.
- Sidewalks are tired.
- It's difficult to get people out of cars.
- The bus shelter is a hang out.
- The entry to Baker Street needs improvement.
- Would like to see a consistent "heritage look".
- How to pay for upgrades?
- Funds for festivals.
- Business promotion is key.



Photos: Workshop participants



Event #2 - May 17, 2016: "Sparking Urban Design Ideas in Downtown Nelson: A Community Workshop"

Participants at the Community Workshop clearly indicated that the intersection of Baker and Ward Streets is the heart of the downtown. This inspired the development of a pedestrian oriented intersection at Baker and Ward Streets.

Following a presentation on urban design, the participants discussed the following questions in small groups:

- What makes downtown Nelson special?
- What is missing in downtown Nelson?
- What is your vision for downtown?
- What strategies do we need to achieve it?



Photos: Workshop participants

Highlights from Participants' Comments:

- Increase pedestrian priority downtown.
- Consistency for signage, including sandwich boards; Issues with enforcement of the sign bylaw.
- The need for consistent lighting and a desire for more lighting for special events.
- Street Culture on Baker Street is a challenge and there is a need to increase community policing.
- Biking downtown is unsafe and there is a need for more bicycle racks.
- Need for better wayfinding (parking and directions).
- Cluttered and tired sidewalks.
- The intersection at Vernon and Hall Streets is challenging.
- A desire not to spoil downtown. People like its character and uniqueness.
- Need for public washrooms and drinking fountains.
- More public amenity spaces and redesign amenity areas.
- More activities for children.
- Consideration of seniors.
- Improve the lanes, initiate beautification, and reduce number of dumpsters.
- Increase art and decrease graffiti.
- Need more mixed use and residential in the downtown.
- Tram or shuttle bus on Baker Street.
- Park once strategy with good wayfinding.
- Clean up awnings as well as garbage and tagging.
- More evening lighting.
- More benches.
- Need a central plaza.
- Design amenity areas to discourage loitering.
- Owners of private buildings need to maintain them.



Event #3 - June 18-20, 2016: “Design Fest: Hands-On Workshop: Developing Urban Design Strategies for Downtown Nelson”

As part of the Design Fest weekend—a three-day design charrette to create several “made in Nelson” designs for the public realm—there was a community workshop on June 18, which focused on a series of questions grouped into six themes. Small groups discussed these themes:

- Urban design elements
- Public amenity spaces
- Parking
- Lanes
- Signs and wayfinding
- Community partnerships

Event #4 – June 20, 2016: Public Presentation - Conclusion of Design Fest

At the public presentation, the team shared an overall design concept, a streetscape kit of parts that would apply differently to individual blocks and streets in the downtown, as well as a number of sketches that highlighted design opportunities for Baker, Vernon and Victoria Streets. These design concepts include improvements to the intersection at Ward and Baker Streets, a redesign of a mid-block public amenity space, and introducing bulb-outs at street corners. The concepts also highlighted opportunities to improve parking and add greenery.

Highlights from Participants’ Comments:

- Enhancement of amenity areas including increased seating areas; Pop-up green spaces.
- No net loss of parking spaces in the downtown; tracking parking usage.
- Use of the lanes—test one block at a time; green corridors; bicycle routes.
- Wayfinding and signage - directions to businesses, parking, and accommodations.
- Transportation around the downtown – need to accommodate delivery vehicles; Move buses off Baker Street; re-introduce a street car.
- Adopt-a-Block program; competitions for the cleanest block; and add recycling.



Photos: Workshop participants



Pop-up Displays – July and August, 2016: Taking the Urban Design Strategy into the Community

Pop-up displays were set up at the Wednesday markets on Baker Street in July and August, 2016. In between these dates, the display was located at the Nelson Public Library, Touchstones Nelson Museum and at Nelson City Hall.

Citizens were asked to complete questionnaires on streetscape improvements, laneways and sandwich boards. One hundred-eighteen comment forms were collected over the summer. The survey further reinforced some of the comments that were received during the public workshops. The key findings are outlined in Table 1.



Photos: Pop-Up Displays on Baker Street

Table 1: Summary of Questionnaire Results

Big Move	Do Not Support	Somewhat Support	Strongly Support	Downtown Urban Design Strategy
Pedestrian First	15%	21%	64%	A more pedestrian oriented intersection at Ward and Baker Streets
	25%	19%	56%	“Scramble” intersection that would support pedestrian movement across the intersection at Ward and Baker Streets
	5%	18%	77%	Adding street trees to the north of Victoria Street
Improve Public Amenity Spaces	12%	18%	70%	More open space for sitting and enjoying the downtown through improvements to the mid-block amenity areas on Baker Street
	24%	31%	44%	Movable chairs in the mid-block amenity spaces
	12%	17%	71%	Improving existing corner bulb-outs by adding planting, seating and other pedestrian amenities
	18%	22%	60%	Adding more bulb-outs to improve pedestrian amenity areas and safety in the Downtown
	13%	25%	62%	Creating a corner bulb-out at Victoria and Ward Streets
No Net Loss Parking Approach	17%	26%	57%	Create angle parking on Ward Street
Signage	17%	53%	29%	Support the use of sandwich boards in the Downtown
Lane Improvements	4% (No)	11% (Maybe)	85% (Yes)	Would use a lane to walk or bike. The key things that were identified that would make a back lane more appealing were: <ul style="list-style-type: none"> • Green spaces, trees, and planters • Cleanliness/less garbage/less odour • Lighting • Storefronts/street food



2



FRAMEWORK FOR URBAN DESIGN

2 FRAMEWORK FOR URBAN DESIGN

The framework outlined in this section is the foundation for the overall Downtown Urban Design Strategy. This section includes background information to support the recommendations that are articulated in Section 3. It also outlines the street structure and the unifying and distinct elements in the downtown that help to create cohesive, yet unique individual streetscapes. The framework is informed by policy related to pedestrian priority, heritage conservation, vibrancy, multi-modal transportation, parking, accessibility, safety, public amenity space, urban ecology, arts and culture, and wayfinding.



2.1 STREET STRUCTURE AND DEFINING CHARACTERISTICS

There are three primary parallel streets in Downtown Nelson – Baker Street, Vernon Street and Victoria Street. A series of connecting streets, as well as two lanes link the primary streets together. Baker is the central spine and main shopping street. Vernon and Victoria are edge streets that provide a mix of commercial and residential uses. Each of the three primary streets has a unique character.

The intersection at Baker and Ward Streets is the centre of the downtown with high pedestrian and vehicular activity. One block north, the intersection at Vernon and Ward Streets is part of the Highway 3A bypass of downtown and it also has high vehicular and pedestrian activity. Hall Street and Ward Street both connect the downtown to the waterfront.

There are two primary vehicular gateways in Nelson - one at the west end of Baker Street on Highway 3A, and the other at the corner of Ward and Vernon Streets (the Highway 3A bypass). Stanley, Ward, and Josephine Streets are all main connecting streets to the residential neighbourhoods south of downtown.

Baker Street extends eight blocks east from Highway 3A to the residential edge at Cedar Street. A collection of late 19th century and early 20th century buildings, as well as mature trees frame the street and a wide variety of shops, services and hotels line the street giving it a diverse and interesting feel. It is vibrant and could be considered one of the healthiest commercial streets in British Columbia, if not in Canada. Improvements to the urban realm will further increase the vibrancy of this principal shopping street.



Photo: Sunset on Baker Street



Vernon Street runs parallel to, and one block north of Baker Street. The eastern section has a central median that is planted with mature trees. The western section is part of Highway 3A. Vernon Street serves as an arterial street and a secondary commercial street with shops, offices, the post office, City Hall, the Provincial Courthouse, hotels, a movie theatre, and the Civic Centre. Vernon Street's eastern section is lined with historic residences and a mix of land uses. Further public realm enhancements and redevelopment will help enhance the street.

Victoria Street is one block south of Baker Street and it functions as a transitional street to the residential area south of downtown. This street has a mix of shops, offices, and larger institutional buildings in its core area, which transition to residential uses at the east and west ends. Victoria Street has a pleasant subdued character. Narrow sidewalks and a narrow street right-of-way limit further pedestrian expansion and street tree planting.



Photo (above): Looking west on Vernon Street



Photo (above): Looking west on Victoria Street



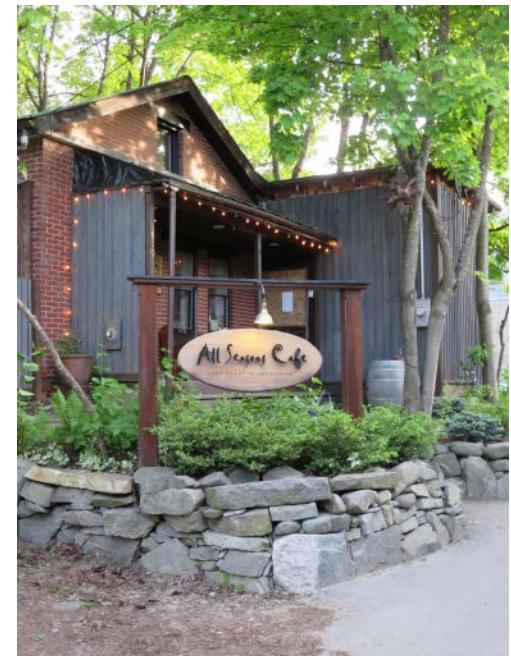
Two lanes located to the north and south of Baker Street provide access for commercial deliveries, as well as space for waste facilities, utilities and parking for downtown businesses. The surfacing in the lanes is variable and they are a low priority for snow removal in the winter.

A few small businesses front onto the lanes. Murals have been painted on building walls and string lights have been installed to add interest and vibrancy.

There is potential to transform these lanes into pedestrian and bike routes that would connect through the entire downtown. It is also possible to envision more programming of these spaces and the addition of public art, gathering spaces, and more businesses fronting on the lanes, or having main street and back lane access.



Photos: Herridge Lane



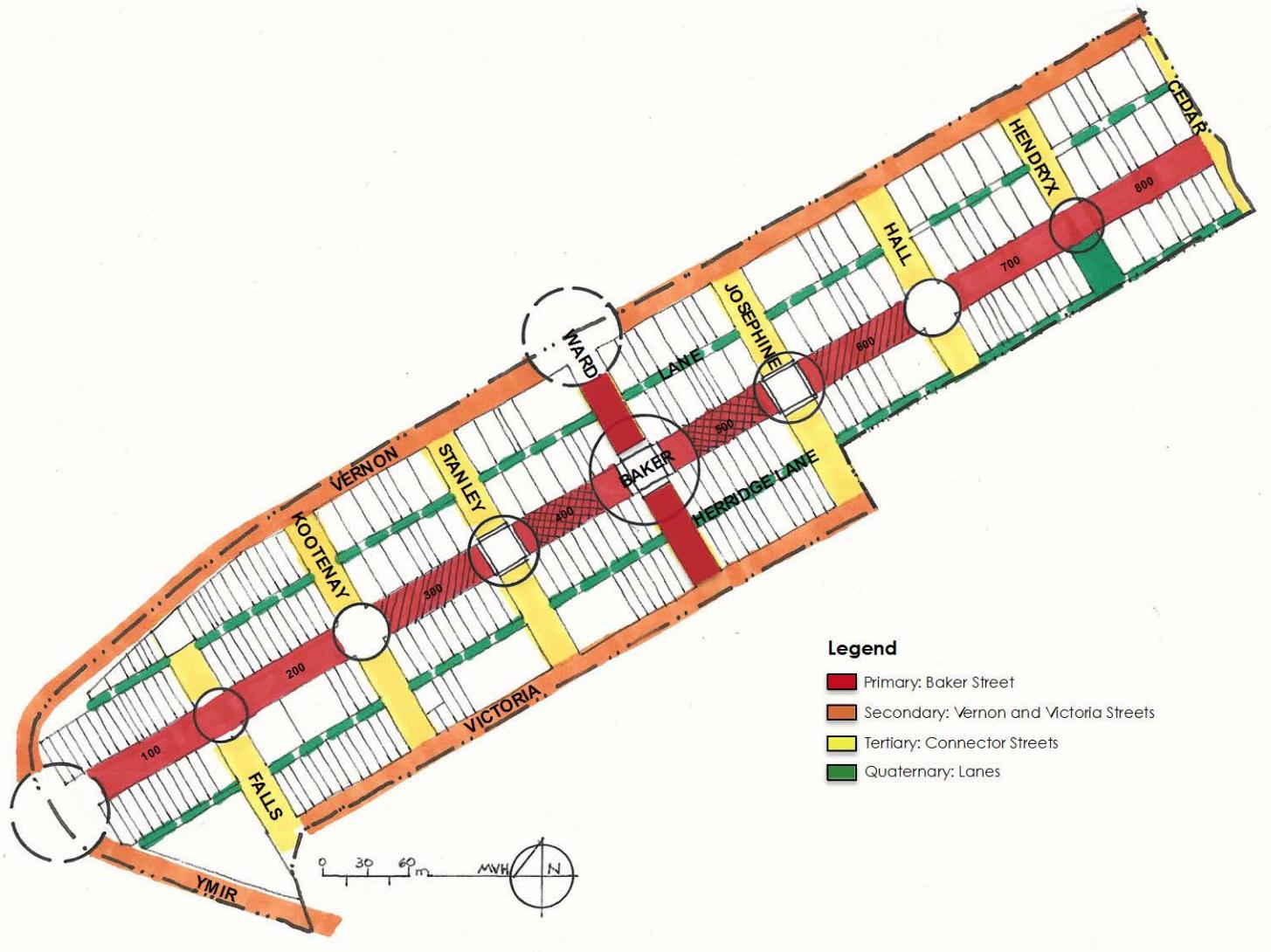


Figure 5: Street hierarchy in the Downtown including Primary, Secondary, Tertiary, and Quaternary



2.2 UNIFYING & DISTINCT ELEMENTS

There are both unifying and distinct elements throughout the downtown. The *unifying* elements provide continuity and a visual connection between the three downtown streets – Baker, Vernon and Victoria. In contrast, the *distinct* elements add character and a unique signature to the public realm.

The elements are outlined in the table below. They are discussed in detail in Section 3:

Table 2: Unifying and Distinct Elements

Unifying Element	Distinct Element
Sidewalk zones	Lanes
Secondary wayfinding markers	Murals
Street furniture	Primary gateway signs
Lighting	Premium paving
Street trees	Public art
Planters	Transit station shelter
Bike shelters	Bulb-outs
	Baker Street amenity areas
	Outdoor patios
	Washrooms
	Awnings and canopies
	Business signage
	Utility boxes with art designs
	Locally designed benches
	Business-owned planters

The summary pages that follow detail the *unifying elements* for:

- **Baker Street** – with a heritage style application;
- **Victoria and cross streets** – with a transitional style; and
- **Vernon Street** – with a contemporary style.

A summary page of the *distinct elements* is provided on p. 20.



Baker and Ward Street *Heritage Style*

Benches

Bike Racks

Waste

Tree Grates

Bollards

Planters

Lighting

Option 1



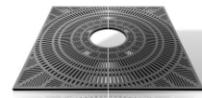
Canaan
CAB-820 Bench



Wishbone Ltd.
Classic 2 Space
Bike Rack



Maglin
MRC 202



Urban Accessories
Harris Tweed Tree Grate



Sternberg Lighting
Parkside 4555LED



Crescent Gardens
Dorchester Planter



Lumenarea
Victoria V1CB80

Option 2



Wishbone Ltd.
Classic Park Bench



Dero
Custom Hoop Rack



Canaan
CAR-182 Recycling
Station



Urban Accessories
Maple Leaf Tree Grate



Maglin
MTB 650



Barkman Concrete
Santa Fe Planter



Lumenarea
Prestiga 3690

Bike Shelters



Wishbone Ltd.
Bike Rack Shelter



Dero
Pocket Shelter

Water Fountains



Haws
Model 3511FR



Haws
3377FR
with Bottle Filler
Attachment

Figure 6: Options for heritage style design application



Victoria + Cross Streets *Transitional Style*

Benches

Bike Racks

Waste

Tree Grates

Bollards

Planters

Lighting

Option 1



Maglin
HBSP Bench



Anova
Arch Powder Coated
Bike Rack



Equiparc
EC3700-RU-Double



Urban Accessories
Maple Leaf Tree Grate



Maglin
MTB 650



Barkman Concrete
Santa Fe Planter



Philips Lumec
New Westminster
Series

Option 2



Victor and Stanley
CM 324 Bench



Wishbone Ltd.
Classic 2 Space
Bike Rack



Canaan
CAR-182 Recycling
Station



Urban Accessories
Harris Tweed Tree Grate



Sternberg Lighting
Parkside 4555LED



Crescent Gardens
Brunello Rolled
Rim Planter



Philips Lumec
Domus 55 Series

Bike Shelters



Water Fountains



Haws
Model 3511FR



Haws
3377FR
with Bottle Filler
Attachment

Figure 7: Options for transitional style application



Vernon Street *Contemporary Style*

Benches



Maglin
MLB 1200 Bench

Bike Racks



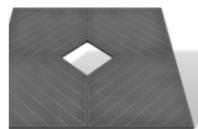
Dero
Bike Bike Rack

Waste



Equiparc
EP 3520-RU-Double

Tree Grates



Urban Accessories
Variations Tree Grate

Bollards



Custom Existing
City Spec

Planters



Victor and Stanley
Urban Square Planter

Lighting



Custom Existing
City Spec

Option 1



Equiparc
EP 1980 Bench with LED Illuminators



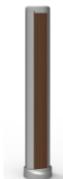
Equiparc
EP59 with LED



Maglin
LXRC1503-48-MS



Equiparc
EP 6934 Tree Grate



Maglin
SCTB1600



Barkman Concrete
Contempra Planter



Philips Lumec
Domus 55 Series

Option 2



Forms & Surfaces
Knight Bench



Wishbone
Turisno 2 Space



Forms & Surfaces
Apex



Custom Existing
City Spec



Custom Existing
City Spec



Custom Existing
City Spec

Option 3*

* Existing Hall Street Application Style

Figure 8: Options for contemporary style application



Distinct *Elements and Spaces*

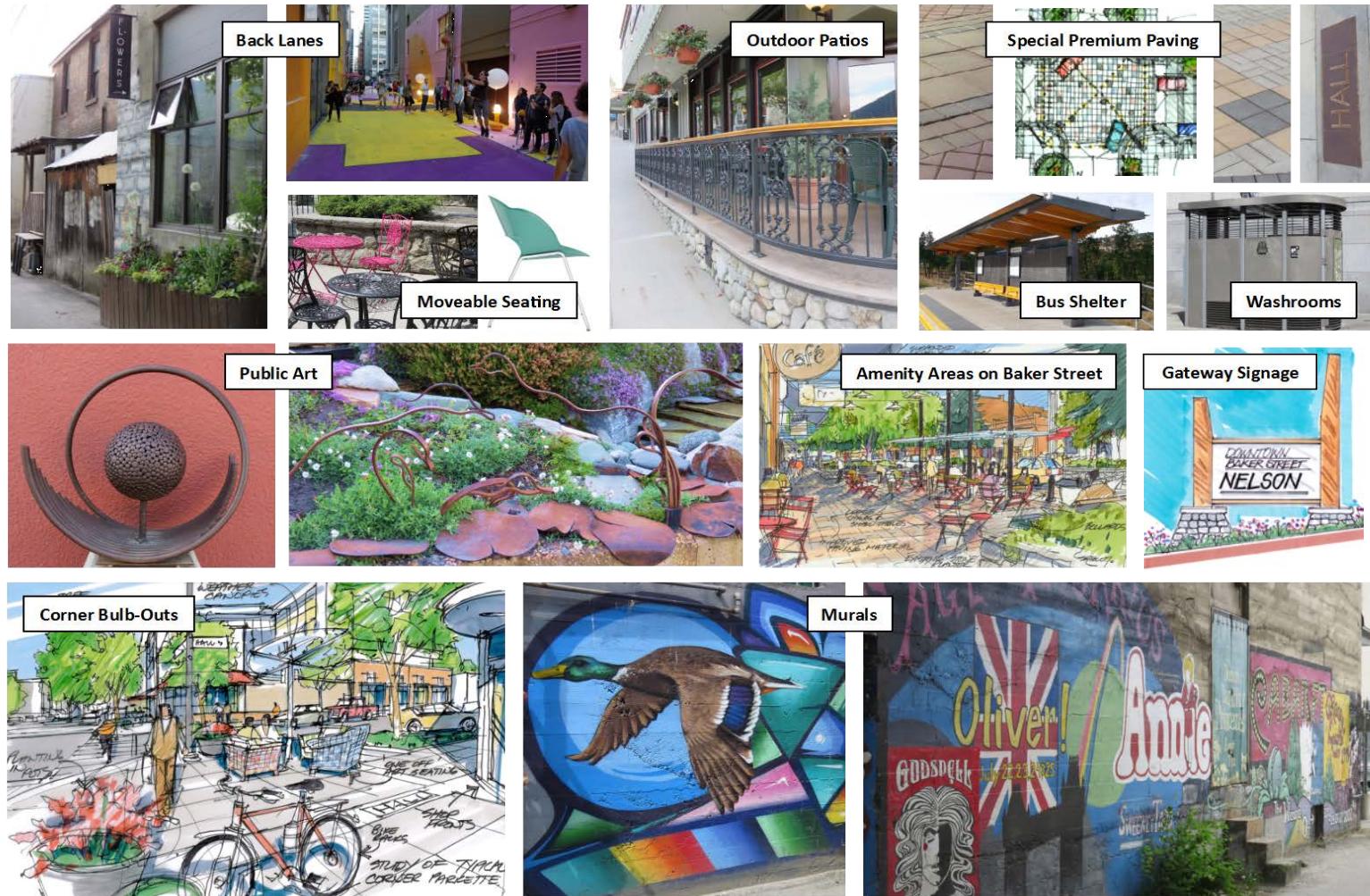


Figure 9: Distinct Elements and Spaces in the downtown



2.3 PEDESTRIAN PRIORITY

Making pedestrians a priority in the downtown was a recurring theme throughout the public engagement process. Pedestrian priority means that walking is prioritized over other modes of transportation such as cars. Strategies to ensure that pedestrians take priority include improved street crossings, more public amenity areas, additional seating, better lighting, lower traffic speeds, pedestrian clear zones on sidewalks, and bulb-outs.

City policies and plans emphasize the need for more pedestrian activity in the downtown and they provide an important framework for this strategy.

The *Sustainable Waterfront and Downtown Master Plan (SWDMP) (2011)* specifies that the downtown will be a pedestrian priority area with clear gateways, gathering places and sight lines. Improving connections for pedestrians is a clear focus in the *Active Transportation Plan (2010)*, and the *Official Community Plan (OCP)*, reinforces the City's commitment to sustainability principles including healthy neighbourhoods, robust ecosystems, prosperity and resiliency.

Key Recommendations for Urban Design:

- Design a scramble at the intersection of Ward and Baker Streets.
- Create a clear walking zone for pedestrians on sidewalks.
- Increase pedestrian lighting for visibility and safety.
- Enhance wayfinding signage to help direct people to key businesses and places in the downtown.
- Improve amenity areas with new seating and plantings, and create new bulb-out amenity areas on corners.
- Add new street furniture.
- Integrate new amenities including a public washroom and a new transit station on Victoria Street.
- Encourage public art, murals and lighting in the laneways to provide additional routes for pedestrians.



2.4 RESPECT FOR HERITAGE

This strategy supports the City's 35-year commitment to heritage conservation since Nelson was selected as one of the first Main Street communities by the Heritage Canada Foundation (now the National Trust for Canada) in 1981. During the 1980s, significant financial support from the Provincial and Federal Governments enabled Nelson and community leaders to preserve older buildings and streetscapes. There is now an opportunity to build on this legacy of heritage conservation.

Today, the community values its heritage resources—buildings, streetscapes with block and lot patterns based on a grid plan, signs, trees, landscape features, views and vistas. The eclectic streetscapes in Nelson illustrate the development of a Western Canadian city based on its geographic setting, economic and social history as a mining community, its economic decline in the mid-20th century, and its revitalization in the 1980s. The primary policy documents that guide heritage conservation in Nelson include the *Official Community Plan* and the *Heritage Design Guidelines*, and the *Community Heritage Register Update*.

There are three Municipally Designated heritage buildings in the downtown area—Touchstones Nelson Museum of Art and History, Kootenay School of the Arts, and the Yellow Deli. Beyond these municipal designations, buildings and streets in the downtown that have heritage significance are recognized in the *Community Heritage Register*. A *Statement of Significance (SOS)* for each property articulates their individual heritage value and includes a list of elements that define their character.

The following character-defining elements are applicable to this strategy. They can be found in the SOS statements for a number of streets and one intersection in the downtown.

Table 3: Character-Defining Elements Applicable to the Urban Design Strategy

	Description	Character Defining Elements
Vernon and Ward Intersection	Framed by Touchstones, the Hume Hotel, and the courthouse this intersection was traditionally the centre of Nelson. The strategy will reinforce this intersection as one of the two major intersections in the downtown.	<ul style="list-style-type: none"> • Views to West Kootenay Lake • Boulevard with median • Civic monuments • Tree and shrub plantings • Enclosure of the intersection by historically significant buildings
Baker Street	Baker Street played a significant role in heritage conservation across Canada. This street is also the commercial centre of the downtown. To the extent possible, Baker Street will be enhanced following the character of existing traditional public realm design elements. In reconstructing the street as part of the infrastructure replacement, concrete planters faced with stone that are deemed as character defining elements may have to be removed. Some mature trees will be removed as well.	<ul style="list-style-type: none"> • Spatial definition by architecturally significant historic commercial buildings • Major retaining walls • Prominent stone and brick heritage buildings • Significant boulevard trees • Elements from the 70s and 80s public realm design—outdoor social spaces, paving, street lighting, parking pattern and signage
Vernon Street	Recognized as Nelson's historic primary thoroughfare, Vernon Street will maintain its prominence within the downtown as a key vehicular spine. Where possible, the central boulevard and the mature trees will be maintained as well.	<ul style="list-style-type: none"> • Location • Ceremonial median • Continuous relationship between the street and specific historic buildings
Victoria Street	Represents the transitional zone between the commercial core of the downtown and the surrounding residential neighbourhoods to the south. Victoria Street will maintain a transitional nature and a traditional look through the application of the streetscape toolkit.	<ul style="list-style-type: none"> • Continuous relationship between the street and specific historic buildings • Gentle slope up Ward Street • Transition street to residential neighbourhoods
Ward Street	Recognized as the principle north/south street in the downtown, Ward connects the downtown core with the waterfront and residential areas.	<ul style="list-style-type: none"> • Slope up from the commercial core • Old creek bed underneath the commercial blocks • Connection to Front Street • Contiguous relationship between the street historic addresses
Josephine Street	Bounded by some of Nelson's most significant institutional and commercial buildings, Josephine Street was named after the wife of a local businessman who had a commercial business on Vernon Street during the creation of the original plan for downtown Nelson.	<ul style="list-style-type: none"> • Slope up from the commercial core • Old creek bed under the commercial blocks • Connection to Front Street • Contiguous relationship between the street historic addresses



Key Recommendations for Urban Design:

- Use street furniture applications, especially on Baker and Victoria Streets that reflect a traditional heritage style.
- Maintain and enhance the digital version of the Nelson Heritage Walking Tour.
- Replace mature street trees with trees that are appropriate for Nelson's climate.
- Use paving materials and colours that respect the heritage character.
- Improve awnings and canopies to create a more continuous heritage streetscape style in the Downtown.



Photo: Historic postcard image of the Tremont Block (Courtesy of D. Jones)



2.5 VIBRANCY THROUGH STREETSCAPE DESIGN

Vibrancy can be defined as the positive energy and feeling created by human activity and interaction. Nelson is known for its unique local character and the funkiness of its streetscapes and lanes. Residents and visitors cherish the many local enterprises, as well as the wide mix of goods and services available downtown. They also appreciate that the downtown is busy during the day and at night. Constant changes to streetscape design through art, music, seasonal events and programming, outdoor patios, plantings, and holiday lighting are key to the vitality of the downtown. Amenity areas that create positive 'sticky places' where people congregate and socialize serve to draw people downtown to support local businesses, festivals, events, and markets.

The OCP supports vibrancy through intensification of the streets, as well as an emphasis on more pedestrian use. To further encourage vibrancy, it is recommended that the OCP be amended to allow for more commercial businesses to front onto the lanes, while at the same time encouraging further development and intensification of the main commercial streets – Baker, Victoria and Vernon. The lanes offer a unique and understated experience, which can work well for certain types of businesses. This has the added benefit of creating more active and inviting lanes for residents and visitors to explore.

Key Recommendations for Urban Design:

- Use design to reinforce the primary downtown commercial streets and their unique personalities.
- Engage local artists to help with the design of street furniture in select areas like the intersection at Ward and Baker Streets and in corner bulb-outs.
- Maintain the weekly downtown market on Baker Street during the summer months.
- Integrate movable bollards into the streetscape design to create flexible spaces. An example of this is the seasonal parking patios.
- Incorporate infrastructure for vendors and buskers including electrical outlets and water connections that can be used during markets and events.
- Incorporate seasonal lighting and banner arms on light standards to enliven the streetscapes and individual buildings.
- Extend cafe and restaurant activity onto the street with more curbside and parking patios.
- Eliminate outdoor retail displays except for on special sidewalk sale days.
- Eliminate sandwich boards throughout the downtown.



2.6 A MULTI-MODAL TRANSPORTATION SYSTEM

Transportation is a key factor in the success of the downtown as the central commercial and service district, as well as a community destination. Supporting various transportation options is an integral part of this strategy that builds on the direction of *Nelson's Path to 2040: Sustainability Strategy*, the *Active Transportation Plan*, and the *SWDMP*.

Path to 2040: Sustainability Strategy

A main goal of the *Nelson Path to 2040: Sustainability Strategy* is to ensure that that various modes of transportation are available to facilitate the energy-efficient movement of people and goods. Objectives to achieve this goal include:

1. Establishing direct and accessible, human-powered transportation that is well connected to transit; and
2. Creating bicycle wayfinding signage and parking facilities.

Active Transportation Plan

The *Active Transportation Plan* outlines existing and proposed routes in the downtown including the *Cycle Circle Tour* on Baker Street and the *Trans-Canada Trail* on Ward Street. Both routes provide main commuter linkages and connectivity to trails outside the downtown. The *SWDMP* recommends that the *Cycle Circle Tour* be extended the length of Baker Street to provide a continuous bike route through the downtown.

Back lanes are another potential network that connect through the downtown and can offer alternatives routes for biking and walking.

Other opportunities to support active transportation in the downtown include increasing the number of bike racks and introducing bike shelters. Bike racks should be selected based on the criteria outlined in the City's *Bicycle Parking Design Guidelines*.

Bike shelters offer weather protection and can help protect bikes from accidental damage by providing greater separation from a sidewalk or parking area.

Transit

Supporting public transportation in the downtown is critical to reducing vehicle use (and traffic congestion). A key recommendation of this strategy is to relocate the existing transit station to Victoria Street. Among other things, this move will create opportunities to improve the amenities offered at the transit station and support more public transit use.





Figure 10: Map showing bike routes through the downtown

West Victoria Street and Baker Street Parking and Bulb-Outs Study

A parking study completed in 2013 reviewed the potential of creating additional parking spaces on the 100 block of Baker Street* and the 300 and 400 blocks of Victoria Street. As part of overall streetscape improvements, the study also examined adding bulb-outs, street furniture and thermoplastic crosswalk markings at intersections in the downtown. The findings from that study are summarized below:

- **200 Block Victoria Street:** Replace no-post barriers with bulb-outs and add wayfinding signage; flip angle parking to north side to align better with drive lane alignment in the 300 and 400 blocks.
- **300 and 400 Blocks Victoria Street:** Convert these blocks to a one way. Replace the parallel parking on both sides of the street in the 300 block with angle parking. In the 400 block remove the parallel parking on the south side of the street and replace it with angle parking on the north side of the street to better align the drive lane between the blocks. An estimated gain of 10 to 15 parking stalls result along this section subject to width of the parking stalls.

*In 2016, the City converted parallel parking stalls in the 100 block of Baker Street on the north side to angle parking. A total of 12 stalls were created.

- **Bulb-outs on Baker and Victoria Streets** were also examined and recommended in various configurations, noting that each intersection is unique and would require detailed design and consideration. The standard curb radius of 7.5 metres maintains a minimum travel lane width of 4.25 metres on Baker Street and 4.0 meters on Victoria Street. The proposed design also included shoreline paving to act as a reference for those with visual impairments, saw cut treatments to new sidewalk areas, new crosswalk letdowns with defined urban braille treatments, crosswalk painting, and planting areas. The designs should also consider locations for accessible parking.

The study concluded that there is adequate width on most of Baker and Victoria Streets to incorporate angle parking. Street and bulb-out changes could result in additional parking stalls, as well as improve aesthetics, wayfinding, and pedestrian safety. Although there was no comprehensive consideration for relocating the existing transit station from Ward Street to Victoria Street, further study could examine bus routes from Fall Street to Victoria Street and the detailed design requirements of a transit station on Victoria Street.



Electric Vehicle/Car Share Parking

Currently, there are no electric vehicle and/or car share parking spaces in the downtown. Designating priority spaces for sustainable modes of transportation will displace some ‘standard’ parking spaces. However, incorporating spaces for these modes of transportation is in line with the City’s sustainability goals, and specifically the targets for reducing Greenhouse Gas (GHG) emissions.

This strategy encourages a “no net loss” approach to parking whereby the City will endeavor to replace spaces in other locations when they are removed in the downtown to accommodate public realm improvements or facilitate sustainable transportation. For example, if a bulb-out were added to a street corner, resulting in the loss of parking spaces, additional parking would be added on side streets where parallel parking could be converted to angle parking. Some street widths in the downtown allow for angled parking.

Key Recommendations for Urban Design:

- Further consideration of relocating the transit station from Ward Street to Victoria Street. This will permit the design and construction of a new bus shelter that includes a bicycle shelter and a public washroom.
- Incorporate additional angled parking throughout the downtown.
- Use the lanes for active transportation including cycling and walking.
- Locate bicycle racks throughout the downtown, close to businesses and offices. Bike racks should be both functional and aesthetic.
- Implement shared lanes for cyclists and vehicles to support biking in the downtown.
- Incorporate more signage to support a network of cycle routes in and around the downtown including the Circle Cycle Tour and the Trans Canada Trail.
- Expand electric vehicle and car share parking.
- Consider a street car service on Baker Street, or a heritage-like shuttle service in the downtown



2.7 ACCESSIBILITY AND SAFETY

Accessibility

Ensuring that people with walkers, scooters, strollers and wheelchairs can easily access the downtown will create an area that promotes active transportation for a variety of users. The City's Design Guidelines encourage a public realm that incorporates universal access and principles of universal design where possible. In addition, ongoing maintenance and repair of sidewalks in the downtown, and delineation of a clear pedestrian sidewalk zone will help to collectively create a more accessible Downtown.

Safety

Crime Prevention through Environmental Design (CPTED) is an integral part of urban design that promotes the use design strategies that reduce crime and improve safety for people. The Downtown Urban Design Strategy recommends a number of CPTED strategies including improving the amount and quality of lighting in the public realm to minimize dark spaces, improving visibility into public amenity areas, and creating more transparency through business windows, which allows 'eyes on the street'.

Key Recommendations for Urban Design:

- Maintain a clear pedestrian zone on sidewalks.
- Construct curbs that are wheelchair accessible.
- Provide enhanced street crossing treatments in areas with significant pedestrian traffic such as at the intersection of Ward and Baker Streets.
- Provide weather protection for pedestrians with awnings and canopies.
- Ensure crosswalks are clearly marked, including the pedestrian scramble at the intersection of Ward and Baker Streets.
- Improve pedestrian focused lighting in the downtown by increasing the number of light standards, and improving the quality of the light. Also encourage more lighting in lanes.
- Clearly mark bicycle and pedestrian ways in the lanes.
- Clearly mark shared use lanes on select streets.
- Locate and design sidewalk patios so that the clear zone for pedestrians is respected.



2.8 PUBLIC AMENITY SPACE FOR ALL TO USE

Downtown Nelson is a community hub. Informal areas for impromptu socializing around benches or at street corners are an important part of being downtown—day or night. The two prominent amenity areas located in the 400 block of Baker Street should be redesigned to make them more transparent and inviting for all people.

The OCP indicates that restaurants may use up 50% of an amenity space for outdoor dining. Currently, the delineation between private restaurant use and public amenity space is not clear. It is recommended that the amenity areas be redesigned to allow for clear definition of public and private spaces. Further, it is recommended that this transformation incorporate:

- Flexible seating options, including static benches and moveable chairs;
- Weather protection;
- More lighting and better light quality; and
- Plantings.

The proposed improvements build on the direction of the OCP, which is to create unique public spaces at key points for people to spend time that serve as neighbourhood gathering spaces, as well as places to promote art and culture.

Maintenance is an important part of these amenity areas. The *Five Year Tree Management Plan for Lower Uphill and Downtown Areas* notes that some trees have outgrown the space – they are too large for the planting areas, they are damaging business awnings and storefronts, and their roots are impacting underground infrastructure, roads and sidewalks. Pruning and in some cases, replacement will help ensure these trees continue to add value to the amenity areas.

The intersection at Ward and Baker Streets is a focal point – it is the ‘heart’ of the downtown and a gathering place for people. The SWDMP advocates for the reinforcement of this intersection with unique paving, catenary (overhead) lighting, and public art. This strategy supports this recommendation and seeks to further enhance the intersection with bulb-outs that will provide more amenity space for people to enjoy.

Key Recommendations for Urban Design:

- Redesign the public amenity areas on Baker Street to make them more functional and inviting for all people. Incorporate a mid-block crossing between these two amenity areas.
- Make the intersection at Ward and Baker Streets a focal point and gathering place that includes bulb-outs, a scramble intersection, unique paving, and overhead catenary lighting.
- Introduce bulb-outs at select intersections in the downtown.
- Incorporate local artists into the design of street furniture and public art in some bulb-outs.
- Introduce different seating options in amenity areas, including moveable chairs that encourage more informal gathering and interactions.



2.9 URBAN LANDSCAPE ECOLOGY

The trees and plantings downtown are highly valued by residents and visitors. Further greening of the downtown at all levels - from trees to planters and planting areas is important to attaining the goals of the *Path to 2040 Sustainability Strategy* and to mitigating the impacts of climate change. Increasing the tree canopy and adding more planting areas will reduce the *heat island effect** during the summer months by moderating excessive temperatures. It will also contribute to improved air quality and moderation of stormwater flow in the downtown. To advance water conservation, climate appropriate vegetation should be planted.

Trees and planting areas also make a positive contribution to the quality of the walking and biking experience in the downtown. They have the potential to add year-round colour, texture, fragrance and contrast.

Maintenance including pruning, and replacement of mature trees when necessary is important to establishing a healthy balance between a vibrant urban landscape ecology and possible issues relating to reduced safety for pedestrians, root intrusion and impacts to private property. Continued tree planting and replacement along the sidewalks is recommended, as is encouraging colourful and diverse plantings in planters in front of businesses, in seasonal hanging baskets, and in planting areas in bulb-outs.

*An area which is hotter than the surrounding areas. Urban areas are warmer than surrounding rural areas due to human activities such as the use of impermeable surface materials including asphalt and concrete, which tend to absorb more solar radiation and transmit more heat.

The pervious areas at the bases of trees, as well as in planting areas will help reduce excess stormwater runoff in the downtown and provide recharge areas that can provide water to trees and plants over extended dry periods in the summer months. The tree canopy will also catch some of the stormwater and buffer the intensity of stormwater runoff. Retaining and expanding the planting areas and pervious pavements along the sidewalks will help to reduce peak flows during intense storms.

With limited green spaces in the downtown, it is especially important to create linkages to other open spaces in the city, such as City Hall plaza, IODE park, Hendryx Street park, the waterfront (via Hall Street), and Cottonwood Falls Park in Railtown. The east end of the downtown is connected to the lower portion of Gyro Park, which is one of the largest municipal parks in Nelson and provides spectacular views of the city and surrounding areas. Some of these spaces provide the potential for expansion of the local food system. They are all important to connect the urban experience with the natural context that surrounds the downtown.

Key Recommendations for Urban Design:

- Maintain and replace trees, where necessary in the downtown.
- Plant a diversity of species that are native or otherwise appropriate for Nelson's climate.
- Incorporate stormwater management into the design of planting areas.
- Increase trees, shrubs, grasses and other planting.
- Promote rooftop patios and green roof plantings.



2.10 ARTS AND CULTURE

The City of Nelson is committed to fostering a vibrant arts and culture community. In a way, streets, sidewalks, amenity areas, bulb-outs, and lanes become flexible venues for artists to create works and for year-round cultural programming to take place. Nelson also has a thriving culinary scene that includes local cafes and bakeries, as well as pubs and fine dining.

There is an opportunity to capitalize on the local talent, whether culinary or artistic to further express the uniqueness of the downtown. This may include food festivals, art installations, street furniture made by local crafts people, and cultural programming. Using more local artists throughout the downtown will build capacity, as well as a sense of stewardship for the public realm. This, in turn could reduce city maintenance costs and vandalism of public property.

The principles that guide the integration of arts and culture in the public realm are found in the City's policy documents. The Cultural Development Committee plays an important role as the curator of public art, murals, cultural events and heritage conservation.

The *Nelson Path to 2040: Sustainability Strategy* identifies Arts, Culture and Heritage as a focus area and advocates for an authentic cultural fabric. The *Art in Public Places Policy* identifies that street furniture, lights and waste receptacles can be part of public art, and the *Murals, Development Permit Guidelines*, provides a framework for the creation of murals in public places.

Key Recommendations for Urban Design:

- Invite local artists to design murals in the lanes.
- Invite local artists to create public art for the downtown.
- Engage craftspeople and artists to design benches for select bulb-outs.
- Collaborate with the Cultural Development Committee on implementation.
- Explore opportunities to create connections between the downtown and annual food events.
- Make public art in the blocks adjoining Baker and Ward Streets the first priority.

Key Recommendations for Urban Design:

- Encourage murals in the lanes to further enhance these areas and make them more welcoming for pedestrians and cyclists.
- Restrict advertisements and images of products, as well as profanities and violence, exclusive of historic imagery.
- Require artists to develop a maintenance program, and have them agree to apply a non-toxic anti-graffiti coating will be part of installing a mural.



2.11 WAYFINDING AND SIGNAGE

Orientation within the downtown contributes to a positive experience for residents and visitors. The look and feel of the orientation signage provides an opportunity for the City to present an interesting and unified graphic identity. Signage—both directional and advertising—plays an important role in assisting people to find their destination and in promoting commercial activity and social life in the downtown. This Strategy presents three categories of signage:

1. **Primary Wayfinding** includes gateway signs at the entrances to downtown.
2. **Secondary Wayfinding Markers** includes signs on street corners that incorporate block maps and directional blade signage for businesses and local attractions.
3. **Additional Wayfinding** such as digital signboards can be used to display community events and allow for easy changes to business names.

This Strategy builds on the City's Design Guidelines and the regulations in the *Sign Regulation Bylaw*, 2027, 1983, and it includes details on how signage can be integrated into building facades, canopies and awnings, banners and sign posts. The primary goals are to reduce clutter, and to create a cohesive, yet creative approach for wayfinding signage in the Downtown.

Key Recommendations for Urban Design:

- Redesign gateway signage into the Downtown at Highway 3A and Baker Street, and at the intersection at Vernon and Ward Streets.
- Create secondary wayfinding markers with block maps and blade directory signage.
- Create a consistent sign area along downtown storefronts.
- Eliminate sandwich board signage in the downtown. Consolidate signage on secondary wayfinding markers for businesses and local attractions not on Baker Street.
- Prohibit the use of the following types of commercial signs: pylon signs (free standing signs), back-lit sign boxes, billboards, revolving signs, banners, pennants, bunting, flags (other than national, provincial, municipal flags), balloons or other gas filled inflatable devices, roof signs, changeable copy signs, sandwich boards or any other temporary signs.



3

RECOMMENDATIONS FOR URBAN DESIGN



3 RECOMMENDATIONS FOR URBAN DESIGN

This section provides the detailed design recommendations for the individual elements in the downtown including streets and sidewalks, public amenity spaces, outdoor patios, awnings, signage, lighting, street furniture, landscaping, public art, washrooms, and electric vehicle charging stations.

The framework for urban design that was presented in Section 2 and was derived from existing City policy, as well as input for stakeholders and the public forms the basis for the recommendations discussed in this section.



3.1 DESIGN PRINCIPLES

Eight design principles guide the recommendations that are detailed in this section:

Place-Keeping: Reinforce Nelson's unique and historic downtown character by carefully integrating improvements into the public realm. Continue to embrace art and culture, as well as rich diversity and the creative spirit of citizens in the downtown.

Pedestrian and Bicycle Emphasis: Consider pedestrians and cyclists as a top priority in the downtown while still considering convenience, parking, and access for vehicles.

Uniqueness: Support Nelson's diverse and eclectic local signatures as seen in shop displays and creative design expressions within a set of guidelines and regulations that permit some flexibility.

Connections: Encourage and expand a variety of transportation connections through the downtown with reduced emphasis on vehicles. These may include shared bike lanes and transit. Consider the potential of downtown small bus shuttles as well as the possibility of reintroducing the historic streetcar.

Resiliency: Select street furniture, street lighting, paving, and other physical improvements that require minimum maintenance, add to the economic prosperity of businesses, are durable under the variable climatic conditions of Nelson, and fit within the downtown's existing context.

Vibrancy: Support regular street programming, temporary closures, and activities including performers and buskers that add excitement and vitality the streets. Reinforce the downtown as the place to be during the day and night, and on all days of the week.

Green Streetscape: Improve the streetscapes with expanded planting, and encourage the greening of roofs and terraces of new developments so that local landscape ecology is an integral part of downtown improvements.

Participation: Include local fabricators and artists in the development of street furniture, and public art and murals so that the unique character in the downtown is reinforced.

Design Principles

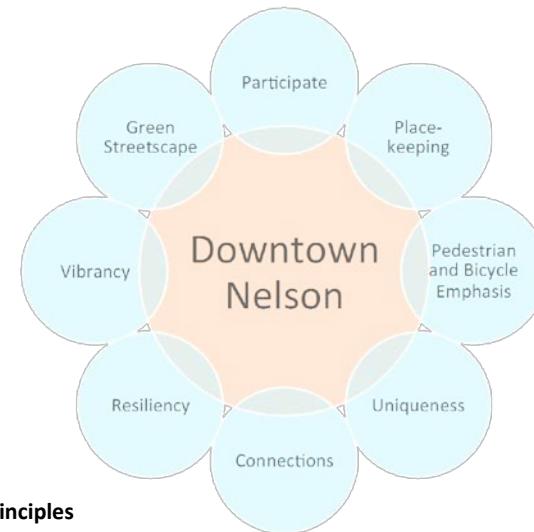


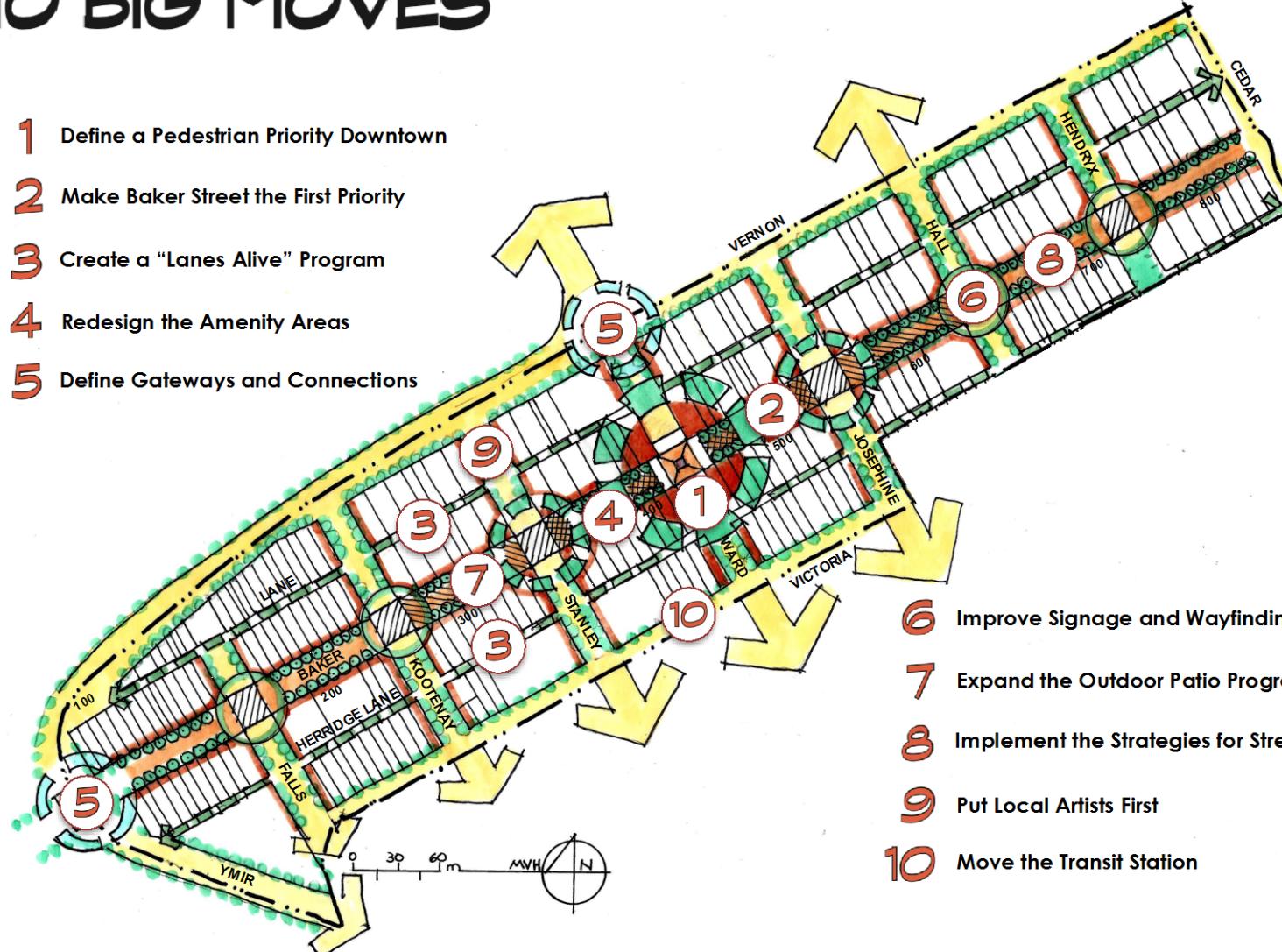
Figure 11 – Design Principles



3.2 10 BIG MOVES

10 BIG MOVES

- 1 Define a Pedestrian Priority Downtown
- 2 Make Baker Street the First Priority
- 3 Create a "Lanes Alive" Program
- 4 Redesign the Amenity Areas
- 5 Define Gateways and Connections



- 6 Improve Signage and Wayfinding
- 7 Expand the Outdoor Patio Program
- 8 Implement the Strategies for Street Furniture
- 9 Put Local Artists First
- 10 Move the Transit Station



1 Define a Pedestrian Priority Downtown

- Ensure that pedestrians come first in planning and design of the streets and lanes
- Develop three sidewalk zones that balance a clear zone for pedestrians with other sidewalk functions
- Design shared bike/vehicle lanes on Baker Street and other principle streets. Post a speed limit of 30 kilometers per hour on these streets
- Expand pedestrian and bike facilities downtown including washrooms, lighting, bike shelters and wayfinding
- Create a pedestrian core area for events on Baker Street that has distinct paving, electrical and water support, and that can be easily closed between Stanley and Josephine Streets (Phase 1) and possibly expand to Kootenay and Hall Streets (Phase 2)

2 Make Baker Street the First Priority

- Reconstruct Baker Street from west to east starting in 2017 or 2018 as part of the necessary infrastructure replacement program
- Introduce bulb-outs where possible to create more pedestrian amenity areas
- Design a “Town Square” intersection at Baker and Ward Streets that includes bulb-outs, a scramble intersection, special premium mosaic paving (granite set pavers arranged in an artistic fashion using various colours), cross-walks, and overhead catenary lighting
- Improve the design of existing amenity areas on Baker Street
- Expand tree, shrub, grasses and perennial flower planting following a “Right Vegetation –Right Place” program

3 Create a “Lanes Alive” Program

- Support more lane business as an affordable alternative to the main streets in downtown
- Introduce a bike and pedestrian program by painting the lanes to define the space
- Expand the mural program in the two lanes in the downtown
- Use super-graphics (large-scale painted art in bold, geometric or topographic designs) and lighting to create an exciting pedestrian and bike “thru-way” through the entire downtown
- Consolidate waste and coordinate deliveries to clean up and improve the unity of the lanes



4 Redesign the Amenity Areas

- Redesign the Baker Street amenity areas to increase visibility, seating, weather protection, and inclusivity
- Create bulb-outs where possible on street corners in the downtown to increase pedestrian amenity areas with seating, planting, locally-made benches and public art

5 Define Gateways and Connections

- Improve the Downtown entrances and intersections at Baker Street and Highway 3A and Ward and Vernon Streets with painted crosswalks and gateway signage

6 Improve Signage and Wayfinding

- Limit signage to two signs per business (one blade sign in addition to building direction signage and window signs)
- Eliminate sandwich boards in favour of landmark directional signs at street corners to improve wayfinding and support a clear zone for pedestrians along the sidewalks

7 Expand the Outdoor Patio Program

- Amend the existing regulations to improve the look and performance of existing sidewalk and parking patios
- Encourage parking patios in the downtown with appropriate landscaping and enclosures

8 Implement the Strategies for Street Furniture

- Coordinate a street furniture program in accordance with the following:
 - ◊ Heritage theme - Baker Street
 - ◊ Contemporary theme - Vernon Street
 - ◊ Transition theme - Victoria Street and cross streets

9 Put Local Artists First

- Expand the overall public art program and placement strategy coordinated with the proposed bulb-outs
- Partner with local artists and fabricators to develop feature street furniture for bulb-out areas
- Consider special installations as part of the “Lanes Alive” program

10 Move the Transit Station

- Relocate the Transit Station on Ward Street to Victoria Street
- Provide tree planting along Victoria Street as well as expanded clear zones for pedestrians considering the average sidewalk width varies between 1.5 metres and 3.3 metres
- Further discussion is needed as the Victoria Street sidewalk is narrow and there are few active edges along the street

3.3 DESIGN RECOMMENDATIONS

3.3.1 Sidewalk definition and building edge

To help define the sidewalk and remove some of the unnecessary clutter three sidewalk zones are recommended. This organization will leave a clear walking zone for pedestrians. As illustrated below, there is an inside building entry area with weather protection (awning illustrated with other potential structures), a clear walking area, and a utility area along the curbside to buffer the pedestrians from traffic and provide efficiency in placing lighting, plantings, snow storage and street furniture.

The standards for the three sidewalk zones are:

- 1. Zone 1 - Building entry zone:** Standard entry zone with a minimum of 0.8 meters. This zone can only include portable planters and seating for a patio. A 1.0 meter horizontal clearance should be kept by building entrances to permit entry and exit. Public art may be permitted.
- 2. Zone 2 – Pedestrian clearway zone:** A minimum clear pedestrian zone of 1.5 meters is required (1.8 metres is desirable). The clear pedestrian zone should take priority in cases where there is limited sidewalk width. No objects can be located in this zone at any time.
- 3. Zone 3 - Street edge zone:** The width of this zone varies with the width of the sidewalk. A general minimum of 1.5 metres is required to accommodate tree plantings and grates, lighting, benches, trash receptacles, bike racks, bollards, water fountains, planters, public art, snow storage, and other utilities. Measurements between street furniture, planting, and lighting in the street edge zone are as follows:

3.3.1.1 Sidewalk Zones

- A minimum of 10 metres between street trees.
- A minimum of 5 metres between trees and light standards.
- A minimum of 1.5 metres between benches and trees.
- A minimum of 2.4 metres between bike racks.
- A minimum of 1.8 metres between the center of a patio table and a street tree.



Figure 12: Sidewalk Zones



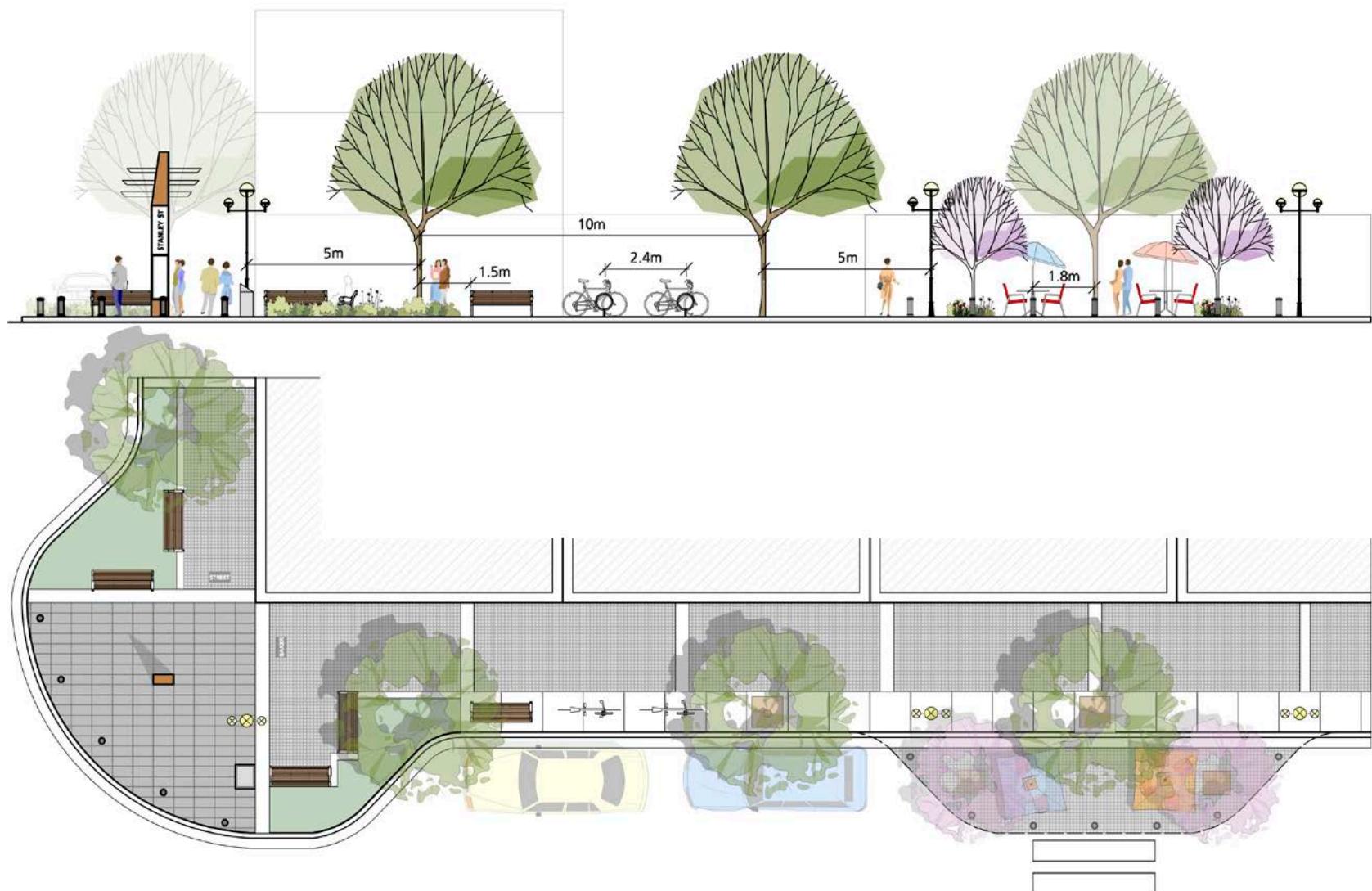


Figure 13: Example of a sidewalk in the downtown that shows placement of signage, street furniture, landscaping and lighting

*Dimensions and distances are conceptual and dependent on utility and block details





3.3.1.2 Building Edge

Conserving the current building fabric is paramount to maintaining the unique character of Nelson's Downtown. The building edge adjoining the sidewalk is important to define the "street wall" and acts as the attractive pedestrian frame to the street experience. Building edge definition through facade articulation, such as varying entry setbacks and building details, is important to create variety and consistency along the street. To enhance the building edge in the downtown, storefront window displays should be maximized to reveal the functions inside (see Section 3.3.6 – Signage for more details). The building entrances should be distinct with planters and benches along the facade near the entrance. Outdoor retail displays should be limited to sidewalk sale days only.

Photo: Example of a welcoming business frontage with a bench and planters



3.3.2 Sidewalks, Streets, Intersections and Lanes

Sidewalks are the lifeblood of public activity and interaction in the downtown. In Nelson, there is a direct relationship between width of the street right-of-way and the widths of travel lanes, parking spaces, and sidewalks. The relatively small block configuration (approximately 90 metres by 90 metres) allows for easier pedestrian circulation between the blocks.

Where there is a generous street right-of-way, there is room for wider sidewalks. Baker Street has an average street right-of-way width of approximately 23 metres. Fortunately, this larger right-of-way offers more room for pedestrians and the possibility for more outdoor patios, street furnishings and street trees. In contrast, Victoria Street has an overall average street right-of-way width of 20 metres and has a corresponding narrower average sidewalk width.

This section examines opportunities to increase the pedestrian priority in the downtown through seasonal or permanent sidewalk expansion, intersection enhancements, street paving treatments, and lane improvements.

3.3.2.1 Sidewalks

The sidewalks in the downtown vary widely in width. Baker Street has sidewalks averaging 3.8 meters, while parts of Vernon and Victoria Streets have narrower sidewalks.

In some cases, there is no room for street tree planting. While Baker Street has a travel lane width ranging on average from 4.0-4.5 metres and street parking priorities (see Figure 14), other streets in the downtown have potential for wider sidewalks. Vernon Street has areas of narrow sidewalks and particularly wide travel lanes. The details applying to sidewalks zones and application are in Section 3.3.8.

Recommendations

- Further study and design feasibility studies are required to determine if some of the downtown streets have the capacity to convert parallel parking to angle parking, widen sidewalks, and narrow the travel lanes based on traffic volumes and other technical requirements. For instance, parts of Baker, Stanley and Vernon Streets are classified as collector streets and may require wider lane standards to accommodate increased vehicular traffic.
- Further design studies should consider widening sidewalks on the west sides of select cross streets including Kootenay, Stanley and Josephine Streets, and include street tree plantings.
- Sidewalk widening should be examined on the south side of Victoria Street from Kootenay to Josephine Streets, as well as on parts of Vernon Street. Sidewalk widening would be subject to relocation of the transit station to Victoria Street, conversion of parallel parking to angle parking and converting the 300 block (and possibly the 400 block) of Victoria Street to one-way as recommended by the *City of Nelson Parking Study* report.



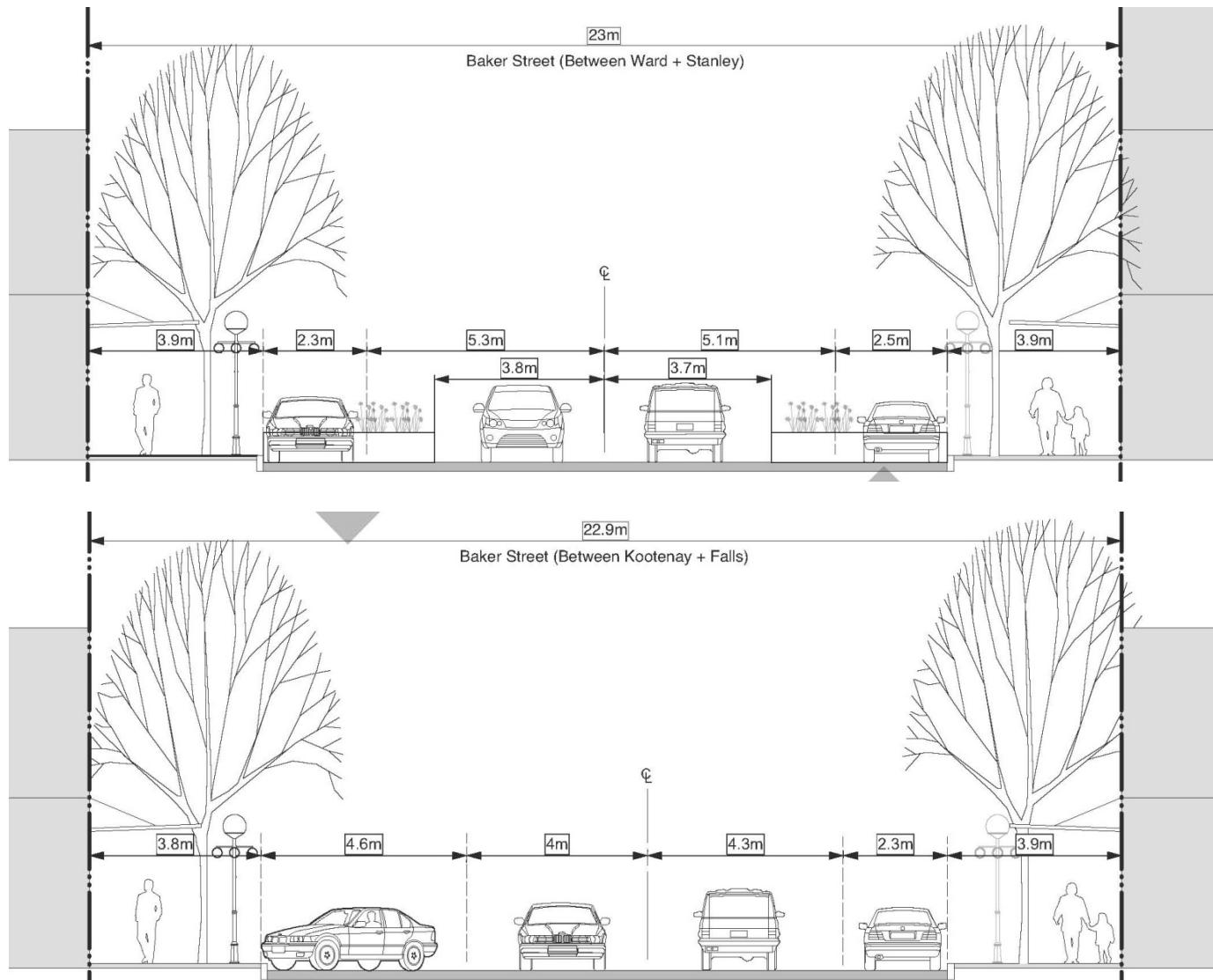


Figure 14: Existing sections through Baker Street illustrate existing sidewalk, parking and travel lane widths that have functional limitations for either expansion or reduction. The minimum travel lane standard width is 4.0 metres. The upper section includes less than the standard 4.0 metres at the public amenity areas, allowing for larger areas that support a pedestrian focus in the downtown.



3.3.2.2 Streets, Parking, and Intersections

Recommendations

A redesign of the public realm on Baker Street is recommended in the short- to medium-term as part of a planned project to update and replace aging sub-surface infrastructure. The following recommendations should be considered at the detailed design stage.

1. **Six Block Core Area:** apply the *premium* design treatment (see Section 4.4) in the core pedestrian area of Baker Street between the 300 and 600 blocks, from Kootenay Street to Hall Street (with the possible same treatment on Ward Street from Vernon Street to Victoria Street). The treatment should include unique paving for the street, additional street furnishings, redesign and improvements of the public amenity areas, and enhanced bulb-outs on each corner (pending further review). The specific design details to consider include:
 - **Sidewalk Treatment:** Cast concrete bands and stone paving with street edge cast concrete curbs.
 - **Bulb-Outs:** Stone paving with cast concrete band treatments through the sidewalk and along the bulb-out edges, new crosswalk letdowns with defined urban braille treatments, and crosswalk painting finishes.
 - **Street Pavement:** Concrete pavers extending onto the street with cast concrete bands across the street and along the outside edge of parking areas to define the travel lane area and reduce the feeling of a defined car area.

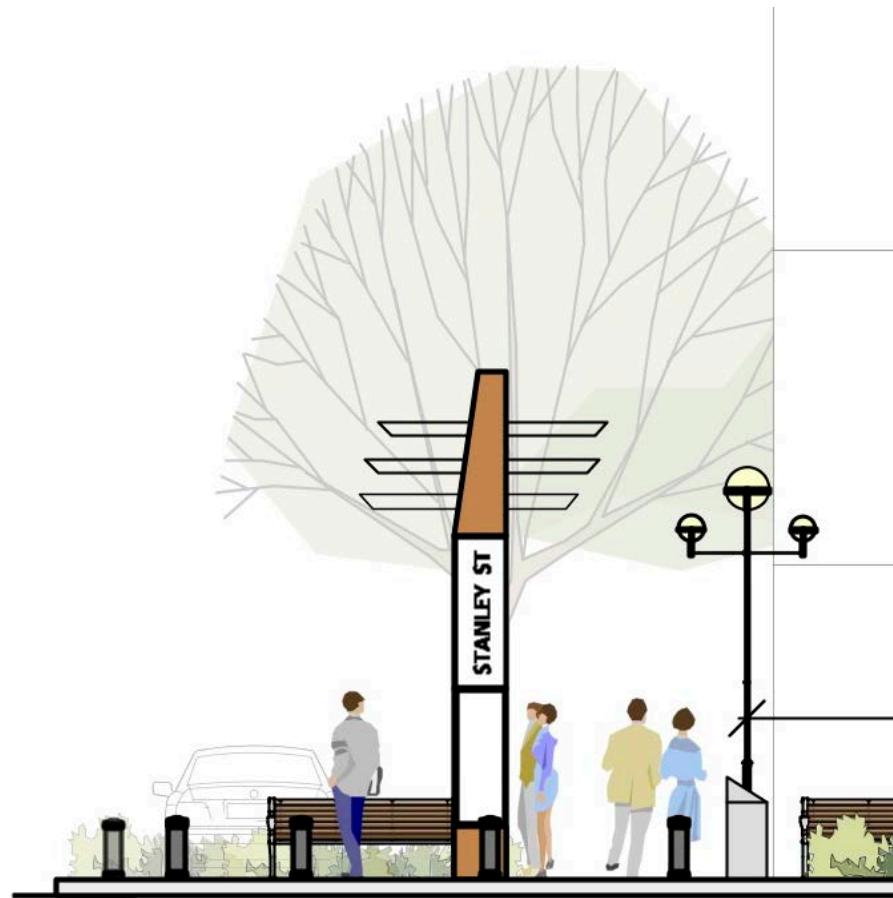


Figure 15: Example of a corner bulb-out with secondary wayfinding marker, lighting, and seating



- **Street Furniture and Public Art:** Should be increased in this area. For example, increasing the number of benches, bike racks, and waste and recycling containers would help to accommodate additional pedestrian needs and create more spaces for civic interaction. A water fountain and public art is also recommended for this area.
- **Public Amenity Areas:** See Section 3.3.3
- **Intersection at Ward and Baker Streets:**
 - **Bulb-outs:** bollards placed at the edge of each bulb-out will create additional amenity space with room for wayfinding signage (see Section 3.3.6.2 wayfinding for details).
 - **Raised Town Square:** in the central portion of the intersection the crosswalks can be a raised platform that will create a distinctive ‘square’ like space. To further differentiate the space, a mosaic can be created by using different coloured concrete pavers or coloured concrete.
 - **Crosswalks:** between the four corners crosswalks can be either stone pavers or coloured/textured concrete to enhance the central mosaic paving. These crosswalks can be raised to the level of the sidewalks and the central square to reduce traffic speeds and create a more pedestrian-oriented environment. It will also define a central ‘square’.
- **Street Lights:** should be coordinated to create a “scramble” intersection where all lights are red at the same time to allow pedestrians to cross diagonally and traditionally (perpendicular to cross streets).

- **Catenary Lighting (overhead lighting):** should be attached to four tall anchor poles (street light pole extensions) located at the corners. This will provide festive lighting at the intersection. Light bulbs can be changed during the Christmas season to red and green to create a special holiday lighting accent.
- **Tree Replacement:** where necessary, existing aging trees should be replaced with large caliper trees (min 9 centimeter diameter) for more instant replacement of volume and effect. Shrub plantings and perennial plantings under the trees in the public amenity areas and at bulb-outs are recommended, except at the Ward and Baker Street intersection due to pedestrian traffic volumes.

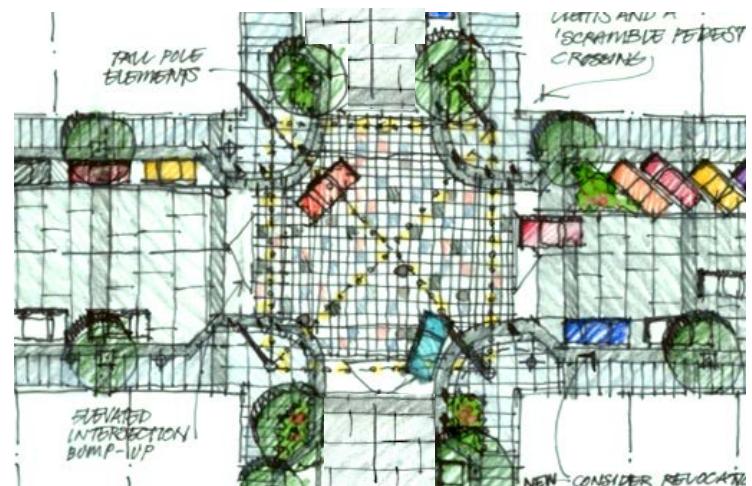


Figure 16: The raised intersection and crosswalks at the Ward and Baker Streets intersection with bulb-outs, a scramble intersection, trees and plantings, and catenary lighting



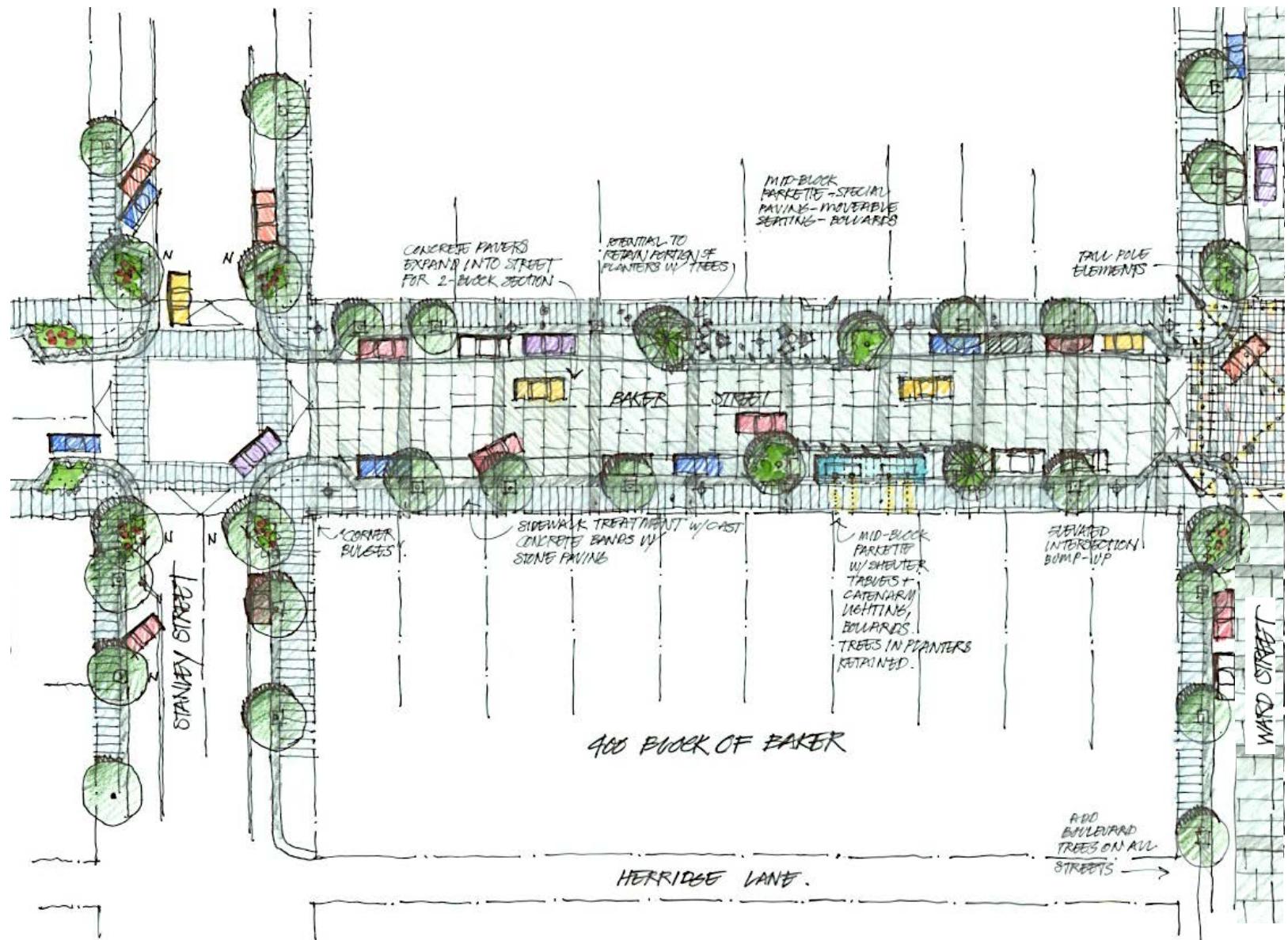


Figure 17a: Redesign concept plan for Baker Street from Stanley Street to Ward Street with the recommendation to expand this treatment east to Hall Street and west to Kootenay Street to complete a four block core section in the downtown.



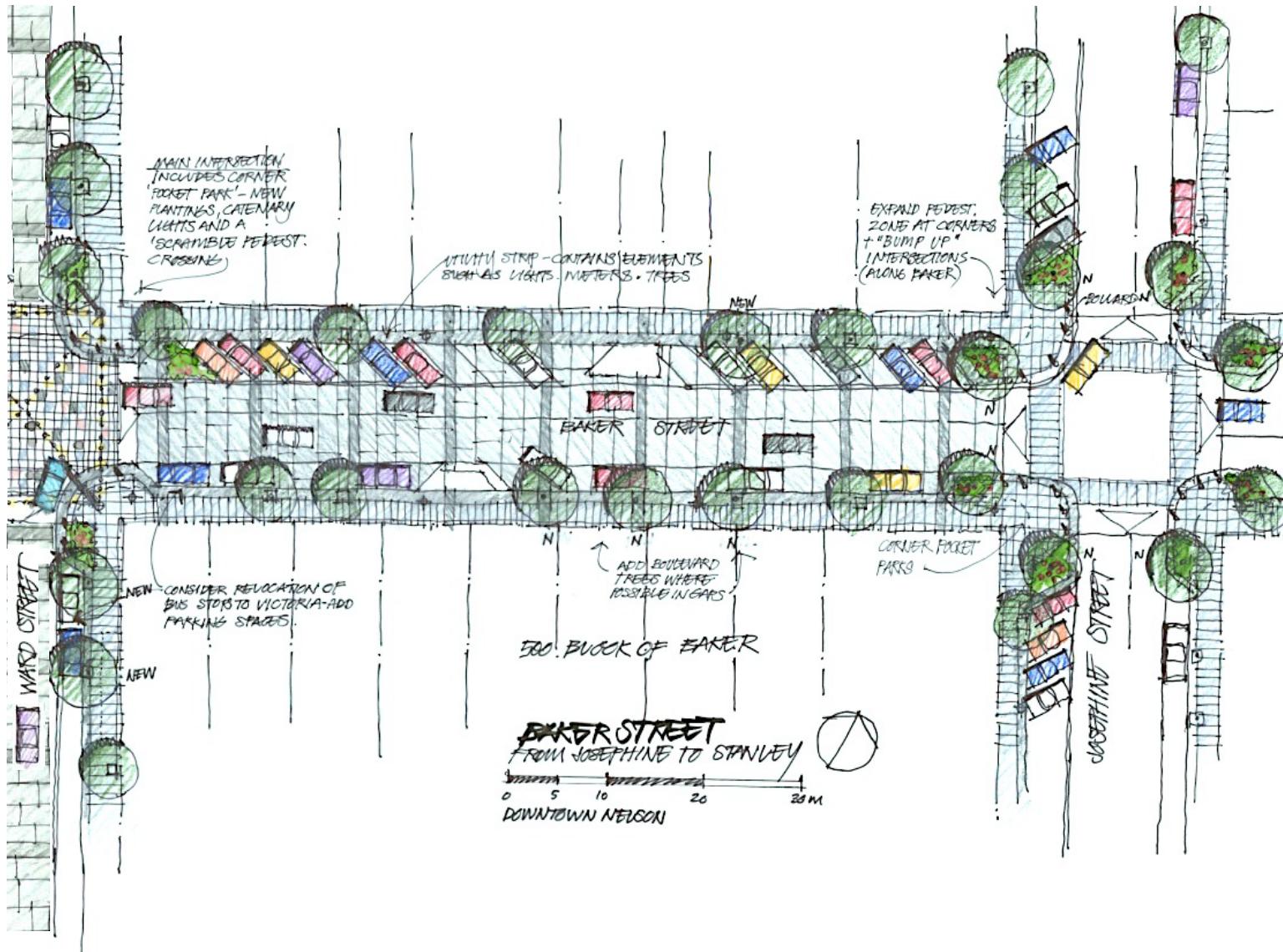


Figure 17b: Redesign concept plan for Baker Street from Ward Street to Josephine Street with the recommendation to expand this treatment east to Hall Street and west to Kootenay Street to complete a four block core section in the downtown.





Figure 18: Redesign of the intersection at Ward and Baker Streets featuring an elevated intersection and crosswalks, unique paving, catenary overhead lighting, bulb-outs, and bollards to define the pedestrian area



2. **Outer Core Area:** apply the *standard* treatment (see Section 4.4) for the outer blocks (100 and 200 blocks, and the 700 and 800 blocks of Baker Street). The bulb-out standards for Hall Street is recommended for this area. Street furniture and lighting should follow the Baker Street heritage application style (see Section 3.3.8 Street Furniture).
3. **Parking Patio and Mid-Block Crossing Option for the 400 Block:** add an elevated parking pad or “parking patio” between Stanley and Ward Streets with removable bollards for a seasonal extension of the sidewalk out to the street.

Existing trees in the planters in the public amenity areas may need to be removed to improve visibility for the mid-block crossing.

During the summer parking will be reduced by 10 parallel spaces and 3 motorcycle spaces. Parking removed from the 400 block should be replaced elsewhere in the downtown on connecting streets or further west on Baker Street, Victoria Street or partially on Ward Street (if the transit station is moved). With the parking patios, the wider sidewalks (approximately 6.7m to 6.9m) can accommodate special events and markets that do not require the street to close down.

4. **Events Plan for the 400 Block with Full Street Closure:** The events plan provides a structured closure option for the 400 block of Baker Street that could be expanded to the 500 block over time. The plan incorporates market stalls, picnic tables for eating, and an outdoor performance area, which could be converted into an evening theatre. Additional electrical outlets and water connections should be considered in the reconstruction of the street.



Photo: Parking Patio built in Quebec City used as a seasonal pedestrian walkway with outdoor patios extending from adjoining businesses



400 Block Baker Street

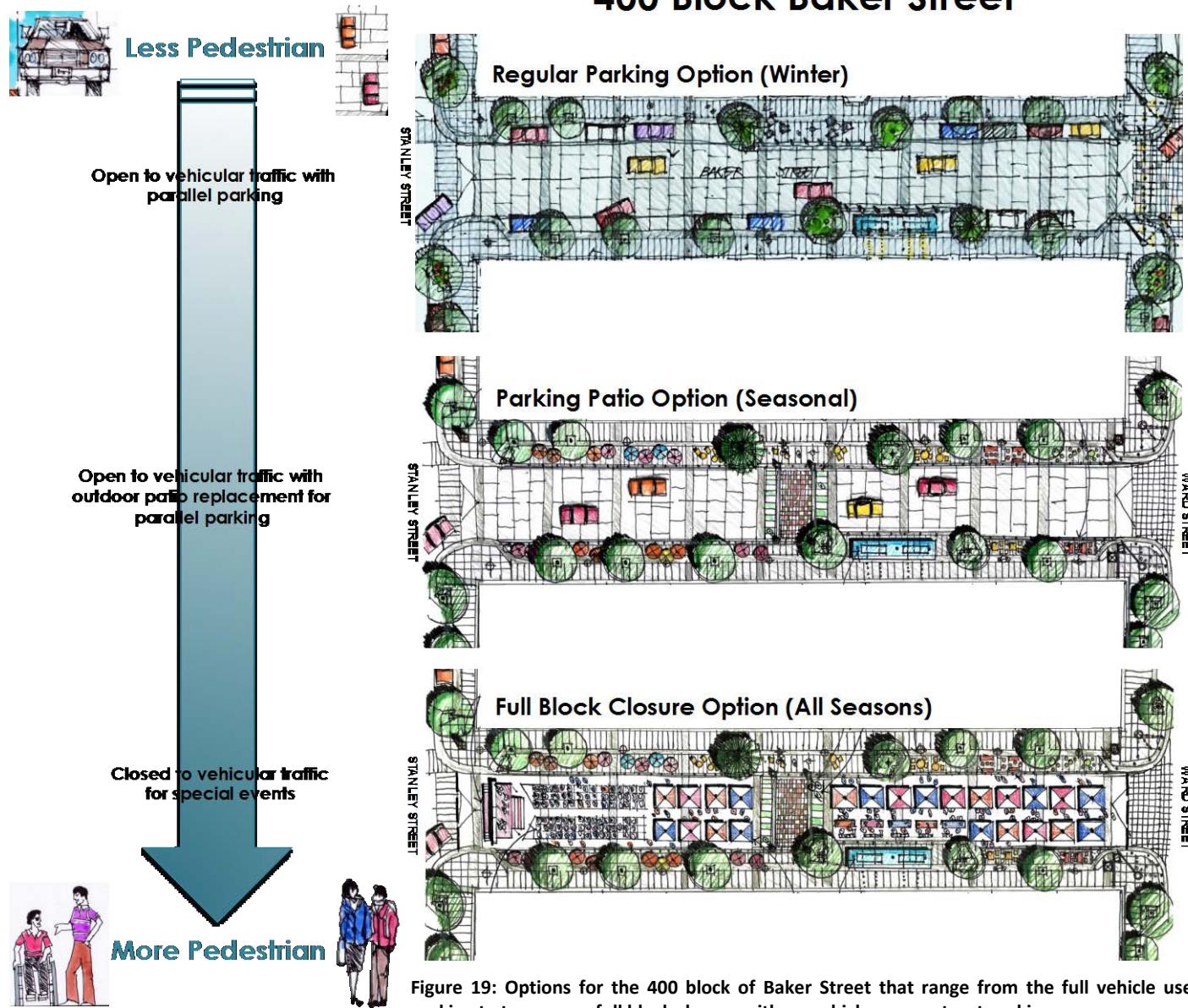


Figure 19: Options for the 400 block of Baker Street that range from the full vehicle use and on-street parking to temporary full block closure with no vehicles or on-street parking

400 Block Baker Street: Parking Patio Option

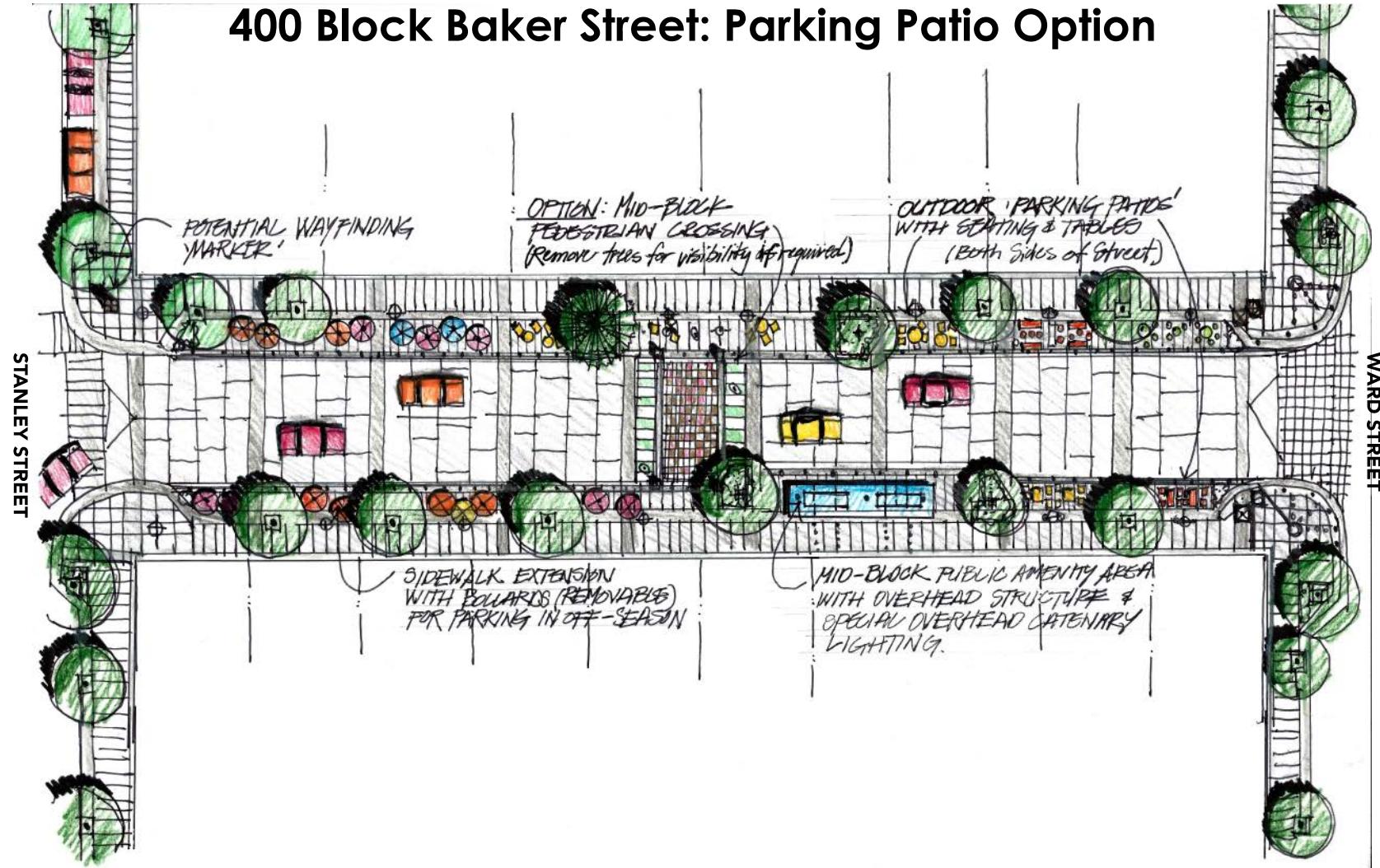


Figure 20: Mid-block crossing in the 400 block of Baker Street with seasonal parking patio option



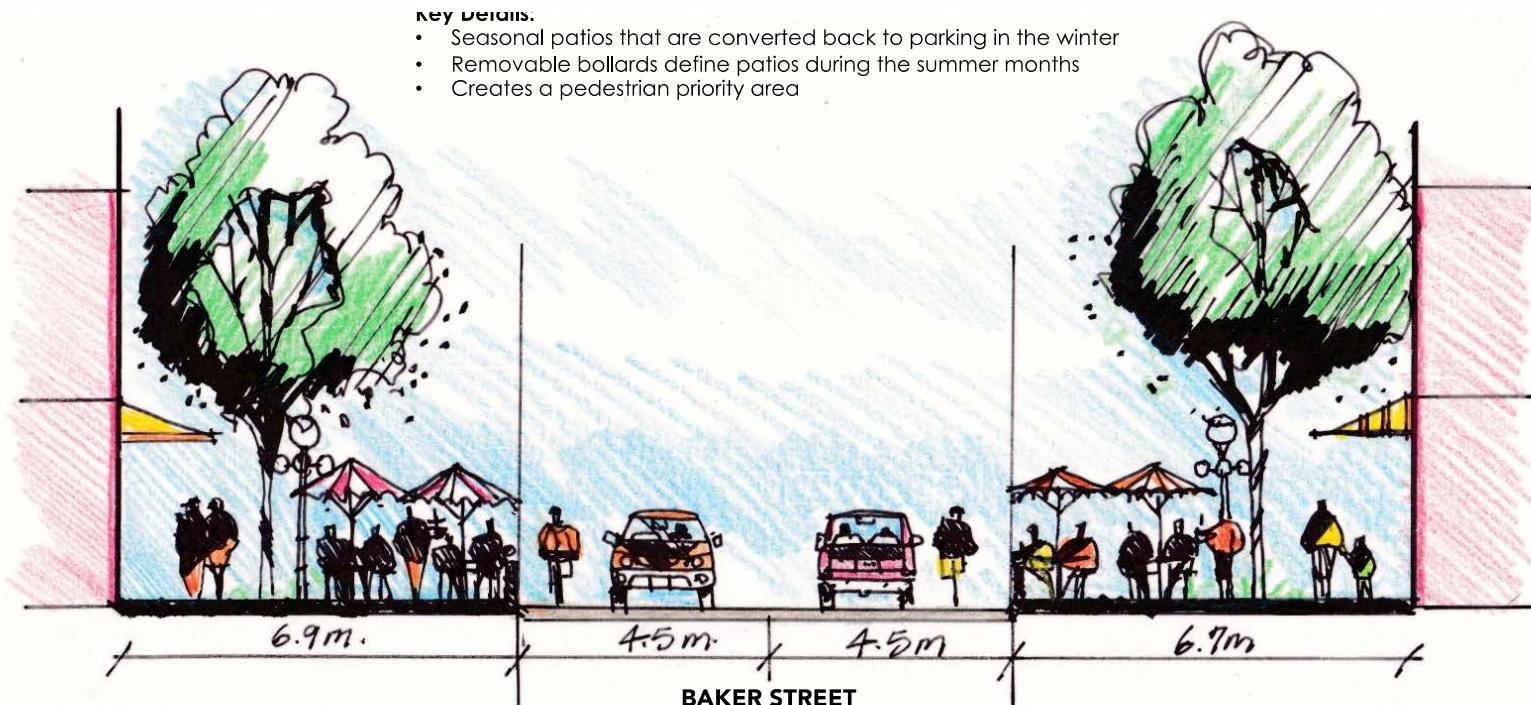


Figure 21: Seasonal parking patio option (elevated parking spaces) in the 400 block of Baker Street

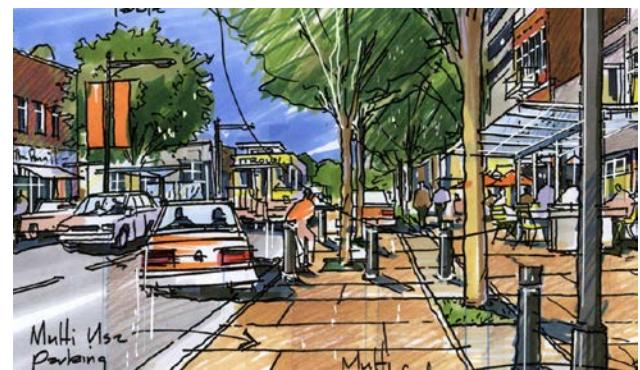


Figure 22: Example of proposed multi-use parking platform with moveable bollards in Lethbridge, Alberta



3.3.2.3 Use of Streets for Special Events

Recommendations

Baker Street hosts many special events. For example, every Wednesday afternoon throughout the summer the 400 block is closed to vehicle traffic for a market. The following recommendations ensure that the proposed design for Baker Street further enhances the pedestrian space

- Extend unique paving across the street to delineate the space as a pedestrian area, thus enhancing the pedestrian priority.
- Install Power outlets on light standards, as well as hardware for catenary lighting at the intersection of Ward and Baker Streets.



Photo: Wednesday market on Baker Street during the summer



- Extend banners over the street at select intersections on Baker Street during events. Pay special attention to the intersections of Ward and Baker Streets, and Stanley and Baker Streets.
- Extending the sidewalk seasonally to include an elevated parking patio with removable bollards to further promote an area for continuous special events that don't require street closure (see Section 3.3.2.2 for detail).



Photo: Live performers at the Wednesday market on Baker Street during the summer

400 Block Baker Street: Events Plan Option with Block Closure

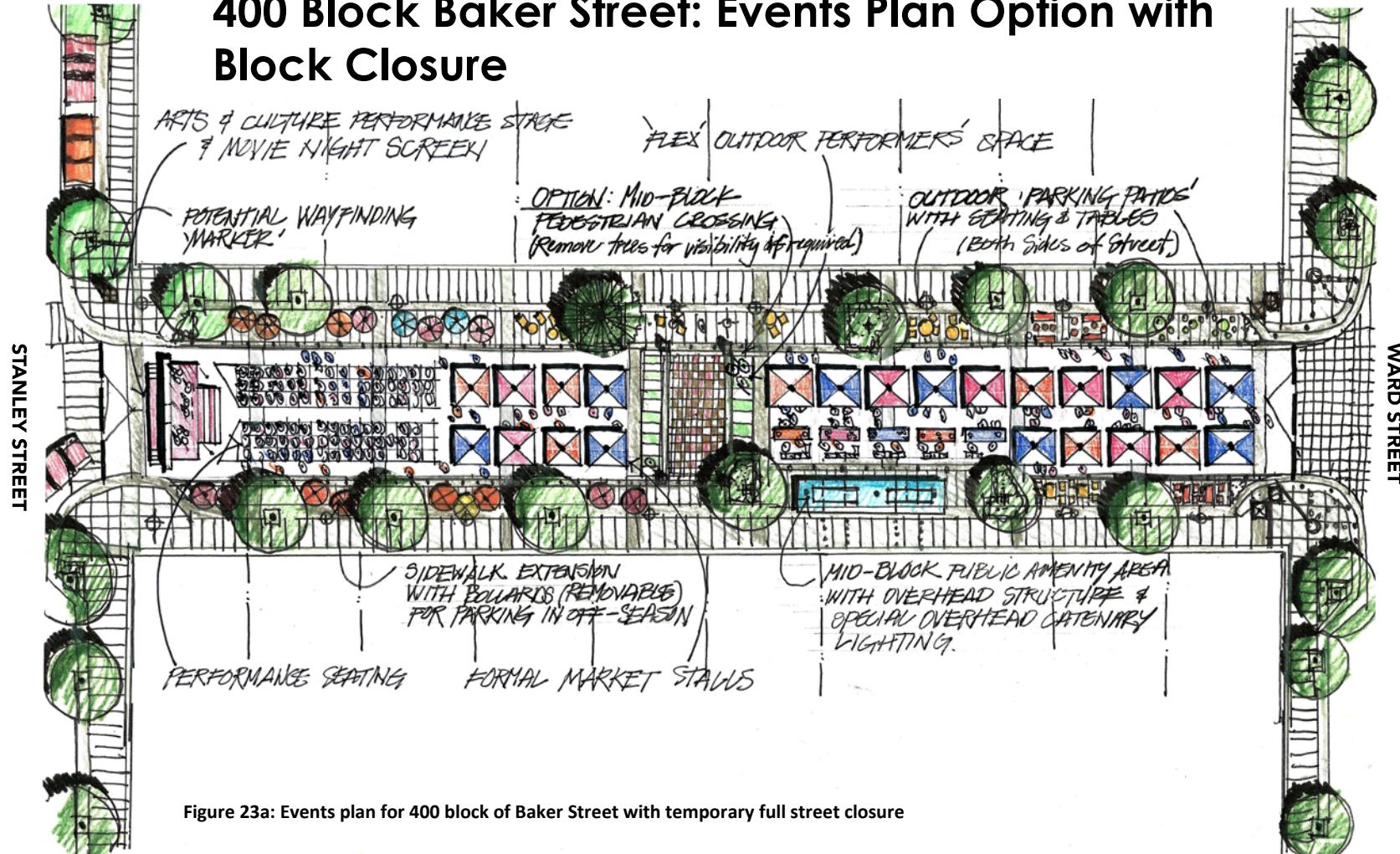


Figure 23a: Events plan for 400 block of Baker Street with temporary full street closure



400 Block Baker Street: Events Plan Option with Block Closure

Key Details:

- Seasonal patios that are converted back to parking in the winter
- Optional stage for community performances or movie night
- Formal market stalls with hook ups to electrical and water
- Creates a temporary pedestrian priority area with no cars

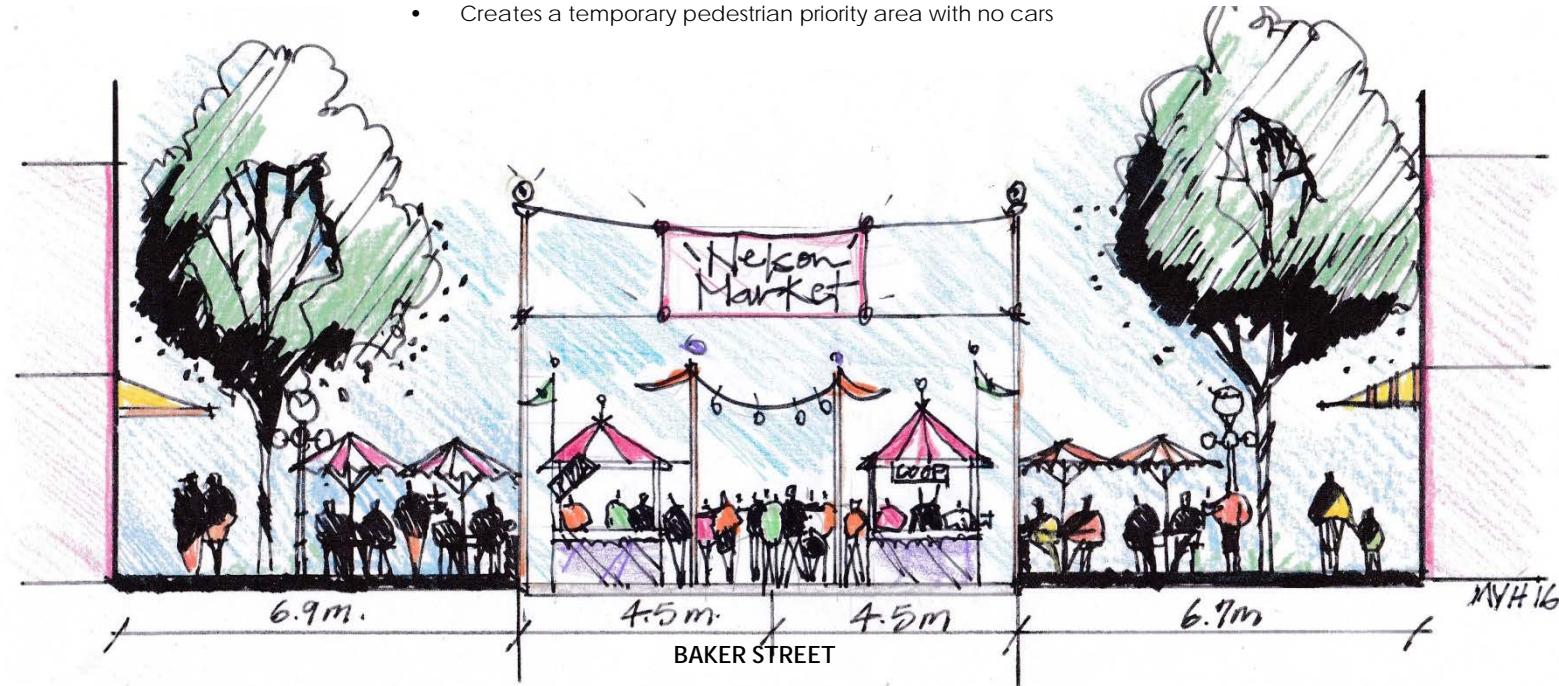


Figure 23b: Events plan for 400 block of Baker Street with temporary full street closure



3.3.2.4 Parking Opportunities in the Downtown

Recommendations

In addition to parking options presented in the *Parking Study Report* (see Section 2.6) angle parking could be added on the north side of Baker Street in the 700 block between Hall and Hendryx Streets, as well as on select cross streets – Ward Street (from Baker Street to Silica Street), the east side of Josephine Street (between Baker Street and Victoria Street) and the east side of Stanley (between Baker Street and Silica Street). Additional parking on Vernon Street may be an option, but this will require reconfiguration of the travel lane widths, or the boulevard.

An inventory of street parking completed in 2016 confirmed that a total of 51 parking spaces could be added to the Downtown by reconfiguring parallel parking to angle parking and filling potential gaps along the street.



Figure 24: Potential bulb-outs at the intersection of Ward and Victoria Streets would add amenity space, seating and tree planting, as well as opportunities for formalized crosswalks and new angle parking



Parking Programming

Review the time limits for parking in the downtown to optimize turnover in the high demand areas. Two hour maximum parking limits in the core could continue while three to four hour parking limits could be considered around the edges of downtown. This would extend visitor stays and provide extended parking where there is less demand. The hourly cost for parking should be affordable and it should not deter people from coming downtown to shop or access services.

Intersection Treatments

Unique crosswalks are recommended at key intersections, including those with high vehicle and pedestrian traffic. Colour imprinted asphalt, coloured and/or textured concrete, or unit pavers can be used.

Section 3.3.2.2 outlines recommendations for design treatments for the key intersections at Stanley and Baker Street, Ward and Baker Street, and Josephine and Baker Street.

Two other key intersections on the periphery of downtown that warrant further investigation and detailed design work are the intersections at Highway 3A and Baker Street, and at Ward and Vernon Streets.

Highway 3A and Baker Street: This intersection is one of the key gateways to the downtown and it would benefit from short-term improvements for both functionality and aesthetics:

- **Entry Signage:** Replace the existing entry sign and reorient it to face the traffic coming from the south (see Section 3.3.6.3 Wayfinding for further details).
- **Traffic Light:** Work with the Ministry of Transportation and Infrastructure to install a traffic light that would replace the four way stop configuration.
- **Pedestrian Crosswalks:** Install stamped concrete or concrete pavers with white painted lines marking the crosswalks to create a distinct heritage connection with Railtown and the downtown.
- **Street Lighting:** Extend the Baker Street heritage lighting across the highway to connect Railtown to the downtown.

Ward Street and Vernon Street Intersection: This intersection is a high pedestrian and traffic node. Recommended improvements include:

- **Primary Gateway Wayfinding Sign:** Gateway wayfinding signs could be placed on the south side of the intersection on both sides of Ward Street to frame the entry to downtown (see Section 3.3.6.3 Wayfinding for further details).
- **Pedestrian Crosswalks:** As in the other principal intersections, the stamped concrete or concrete pavers with white lines will reinforce the importance of this intersection and create a more pedestrian centered node.



3.3.2.5 Lanes

Recommendations

Recommendations for improvements to the lanes include:

- Encourage commercial intensification of the lanes.
- Concentrate commercial uses and murals (public art) in the blocks between Ward and Hall Streets in order to create a critical mass of activity and vibrancy.
- Encourage murals on blank walls to animate the spaces
- Support public art installations
- Encourage business and property owners to paint their back walls with vibrant colours (with the exception of heritage buildings) to add vibrancy and visual appeal.
- Expand wall lighting to create a safe place for walking or cycling during the evening and in the winter when there is limited natural light.
- Coordinate commercial waste and recycling facilities in each block with designated containers for businesses to use.



Photo: Improvements to a lane in Vancouver include adding vibrant colours on the wall and ground plane, as well as basketball hoops and play court markings. The improvements have significantly increased use by pedestrians and cyclists.

- Encourage owners to define their parking areas and to add landscaping to add individual identity.
- Support the lanes as alternative cycling and pedestrian routes, and consider painting a green line through the lanes with bicycle and pedestrian graphics to reinforce their use as viable and exciting alternatives to the main streets.



Photos: Existing businesses and murals in Herridge Lane





Figure 25: Before and after of Herridge Lane including improvements like paint, defined business entrances, murals, bike racks, and seating



3.3.2.6 Bicycle Support in the Downtown

Recommendations

The following recommendations are intended to help make the downtown safer and more bicycle-friendly:

- Make Baker Street a shared vehicle/bicycle route with painted sharrows on the road to encourage cyclists to ride in the travel lanes. This design will ensure that bicycles are part of the pedestrian and bicycle-friendly downtown, especially on Baker Street, which is part of the City Circle Bicycle Route (refer to Section 2.6 for information about existing bike routes).
- Reduce the speed limit downtown to 30 kilometers per hour as is posted on Hall Street to improve safety for both pedestrians and cyclists.
- Construct a separated bike lane on Ward Street, or at a minimum a shared vehicle/bicycle route with proper markings and wayfinding. Separated bike lanes should also be considered on other connecting streets. It is best to consider shared vehicle/bicycle lanes where angle parking exists.
- Develop the rear lanes to function as connectors or alternate routes in the bicycle network.
- Install more bike racks and bike shelter facilities to encourage residents and visitors to travel downtown by bike rather than by car (refer to *Section 3.3.8 Street Furniture* for information on bicycle racks).

Recommended locations for new covered bike shelters in the downtown include:

- The first priority is adjacent to the proposed new transit station on Victoria Street.
- Other proposed locations may include the corner of Hall and Baker Street near the bike shop and at the corner of Baker and Falls Street, again near the bike shop. Both locations would benefit from a partnership with the adjacent businesses to provide surveillance and stewardship of the facilities.
- Electrical outlets (120v) should be installed on bike shelters to charge electric bikes.



Photo: Example of a shared bicycle/vehicle lane



3.3.2.7 Transit Station

Recommendations

It is recommended that further study and community consultation be undertaken regarding the potential transit station relocation from the corner of Ward and Baker Streets to Victoria Street. There are a number of issues to consider including adjoining uses, impacts to the street and neighbourhood, and the design of the space and the transit station facilities.

Minimum required design standards:

- Minimum 4.7m sidewalk width to accommodate the associated transit facilities for adequate public service.
- A driving lane that is 7.1m in the proposed one-way east configuration.
- A transit station length that is approximately 30 to 40m to match the existing length on Ward Street. This length accommodates two transit buses at one time and is equivalent to 5 to 8 parallel parking spaces.

Existing street right-of-way conditions: The average street right-of-way on Victoria Street is 20m.

Adjoining uses and potential impacts: Victoria Street is a transition street where the land use changes from commercial on the north side to a mix of commercial and residential on the south side. Increased vehicle and pedestrian activity associated with the new transit station may be a concern to some businesses and residents in the area.

Setbacks vary for residential and commercial businesses on the south side of Victoria Street. In contrast, the buildings on the north side of Victoria Street have no apparent setbacks from the property lines. The buildings are set directly adjacent to a relatively narrow sidewalk on the north side of Victoria Street.

Transit Station Relocation Option:

1. **Proposed one-way Victoria Street reconfiguration:** The City may choose to make Victoria Street one-way heading east between the 100 and 300 blocks. If the 300 block were to become one-way, existing parallel parking could be changed to angled parking on the north side. A 7.1m travel lane would accommodate a one-way traffic lane, as well as space for buses to stop in the area of the transit station, which would be located on the south side (see Figure 28).

Moving forward with the Transit Station Relocation

There are a number of questions regarding feasibility, land use, community integration, and design that require more study and consultation before an informed decision can be made. At a minimum, this process should include the following:

1. Reviewing bus route requirements with BC Transit
2. Examining the site conditions in detail to determine the requirements for the transit station
3. Refine concept designs
4. Consulting with the adjoining land owners and businesses
5. Consulting the public on location and preferred options





Figure 26: Transit shelter with wood accents, digital information display and glass enclosure (Sourced from Google.com, 2016)



Figure 27: Transit shelter with options for cold weather - glass enclosure, heat and LED lighting. Also includes wood accents and solar panels (Lucid Mgmt Group, 2016)



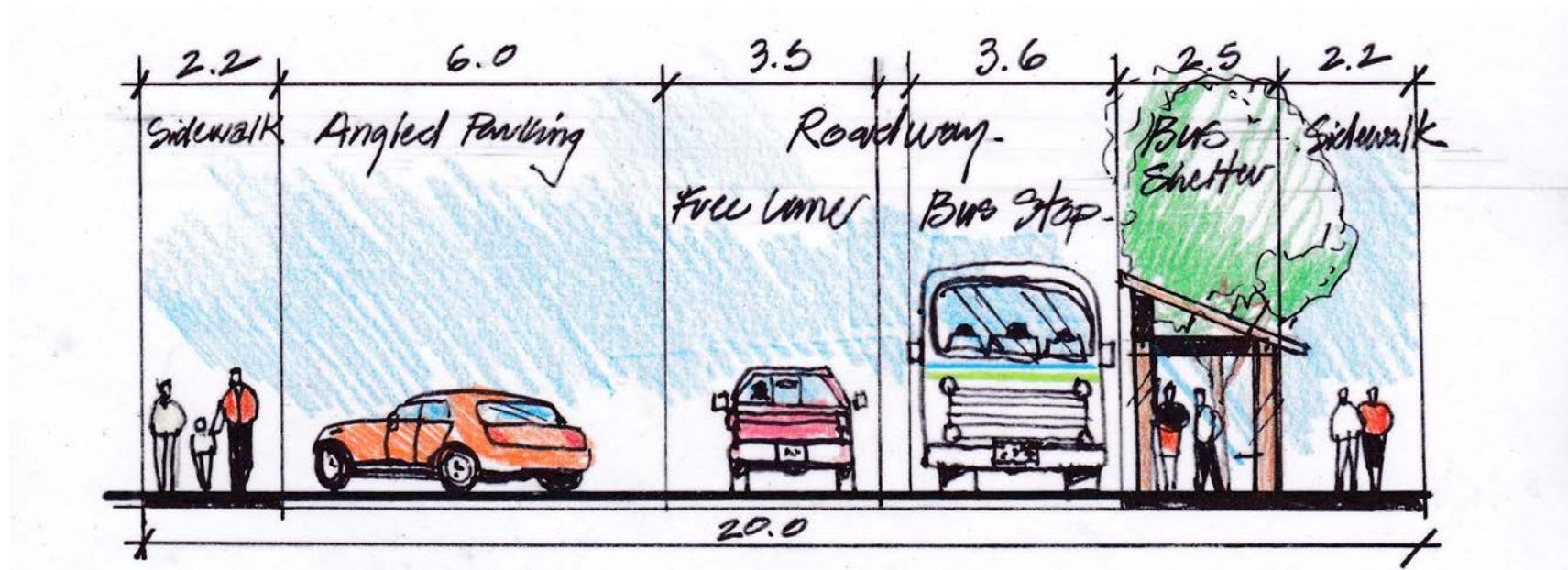


Figure 28: Victoria Street One-Way Option with Transit Station



3.3.3 Public Amenity Spaces

There are a number of public amenity areas downtown, including two prominent areas located on the north and south side of Baker Street in the 400 block. There are similar sized, though less prominent amenity areas on the north and south side of Baker Street in the 500 block as well. All of these amenity areas have large stone walls that separate them from the road and create a clear delineation of the space from the sidewalk.

Smaller amenity areas with similar stone walls are located at the corners of Baker and Kootenay, Stanley, Josephine and Hall Streets.

Additional public amenity areas are possible in the downtown in the form of bulb-outs at street corners. They can accommodate a mix of street furniture, plantings, public art and wayfinding signage. The furniture and public art can be fabricated by local artists and craftspeople.

3.3.3.1 Mid-Block Amenity Areas in the 400 Block

Recommendations

- To improve and further activate these spaces, it is recommended that the stone walls adjacent to the street be removed and replaced with bollards and chains. This will make the spaces more permeable and it will improve visibility for the proposed mid-block crossing.
- Introduce movable chairs and provide an overhead canopy to increase the functionality of these spaces, as well as offer weather protection.

- Integrate overhead catenary lighting and replace the existing paving with a unique paving material.
- Plant grasses and plants around the amenity areas to provide colour and texture throughout the year. Limit annual flower planting to the hanging baskets on the light standards.
- Continue to allow adjacent restaurants to use a maximum of fifty percent of the space for their outdoor patios. The remaining fifty percent should be maintained for public use.
- Remove other stone walls where necessary, but take steps to retain walls where possible for their heritage and aesthetic values.
- Construct canopy with a “community table” to encourage informal activities.





Figure 29: Redesign of the public amenity area on the south side of Baker Street in the 400 block includes movable chairs, a community table with a canopy for weather protection and overhead catenary lighting. The stone walls adjacent to Baker Street are removed.



3.3.3.2 Bulb-out Amenity Spaces

Bulb-outs can provide additional public amenity areas in the downtown and they can help to increase pedestrian safety at intersections, as well as calm traffic and enhance the streetscape aesthetic.

Recommendations

- Conduct further engineering studies to determine the feasibility of adding new bulb-outs in the downtown.
 - Construct bulb-outs over time with the reconstruction of downtown streets.
 - Where practical and appropriate, install public art, seating and plantings in bulb-outs.
 - Commission street furniture and public art from local arts and craftspeople, where possible.



Figure 30: Proposed bulb-out design concept that includes seating, plantings, and space for public art.



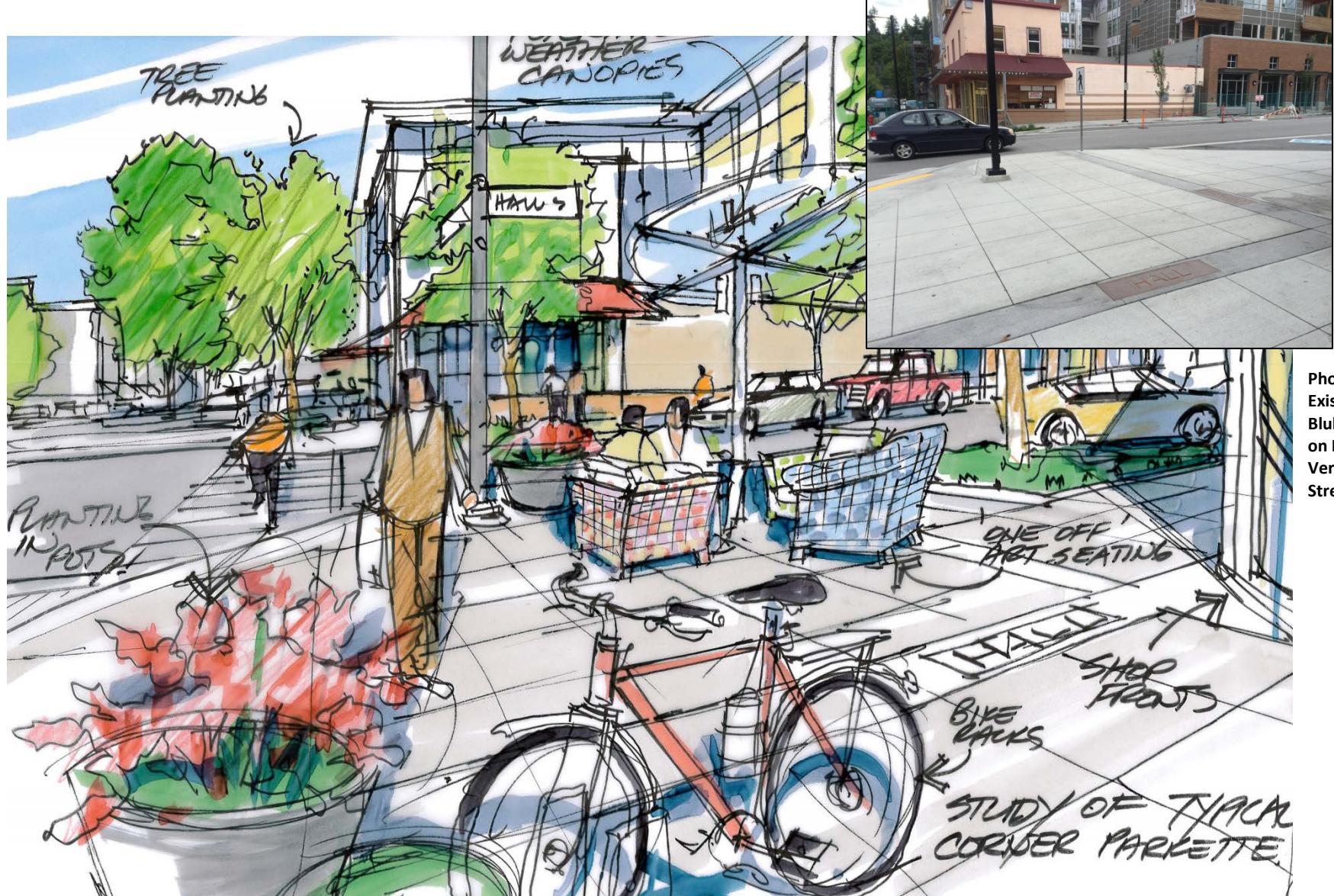


Figure 31: Recommended public amenity improvements for bulb-outs



3.3.4 Outdoor Patios

Existing Conditions

There are some good examples of outdoor patios in the downtown that are unique and designed to fit within confined spaces. Other patios obstruct the sidewalk and constrain pedestrian movement in front of businesses. Some have excessive height and heavy, solid materials have been used for the enclosures, which prevents visual transparency. A number of patios are in need of maintenance to ensure that they are safe and attractive.

Recommendations

- Develop an outdoor patio bylaw that is based on the City's *Outdoor Cafes and Use of Sidewalk and Parking Spaces Policy (5400.00.010)*, but also considers the following:
 - Continue to encourage extended cafe and restaurant activity on sidewalks and streets with seasonal limits between May 1 and October 31.
 - Maximize the use of frontage, while respecting a minimum pedestrian clearway zone of 1.5m (refer to Section 3.3.1.1 Sidewalk Zones for more information).
 - Patio configurations should follow one of the four recommended patio types outlined below: 1) frontage; 2) curbside; 3) parking; and 4) parking or curbside combined.



Photo: Example of a patio with an enclosure that is high and built with heavy solid materials



Photo: Example of a patio that is unique and designed to fit the space



Photo: Example of a patio that obstructs pedestrian movement on the sidewalk



Four Types of Outdoor Patios

Frontage Patio: Located immediately adjacent to the building facade and extending a maximum of 0.8 metres from the building. Frontage patios are effective on streets with narrow sidewalks. A frontage patio could have a simplified application process.

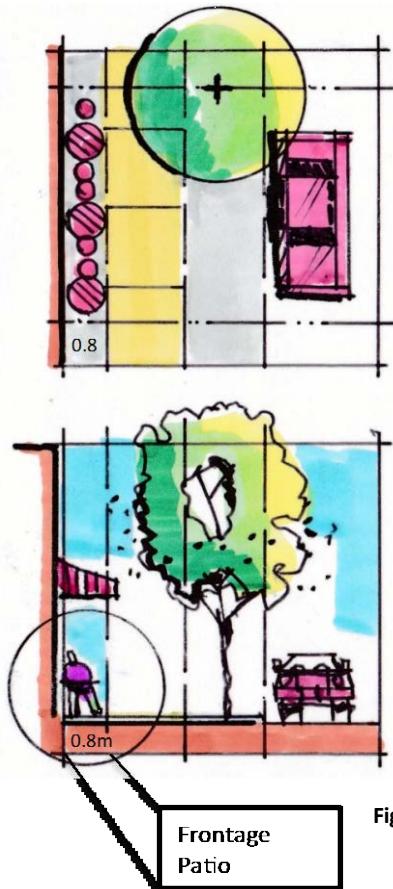


Figure 32: Frontage Patio

Curbside Patio: Occupies the curbside area in the street edge zone (see Section 3.3.1.1) between the curb and the pedestrian clearway. A curbside patio is only feasible on sidewalks that have adequate width to accommodate a 1.5m pedestrian clearway zone and a 0.8m building entry zone. The patio area must be a minimum of 1.5 metres wide and partition is required along the curbside and will also be required on the inside of the curb.



Figure 33: Curbside Patio



Parking Patio: Occupies the parking lane. A minimum of 2.3 metres is required and a partition required on the outside of the curb around the outer three sides of the parking patio.

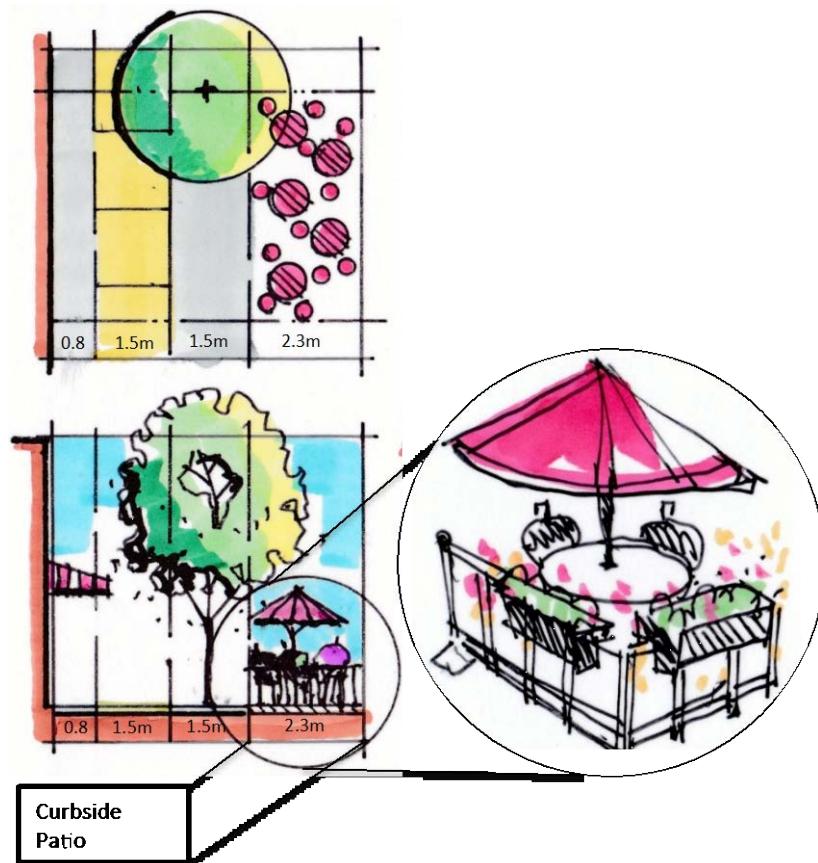


Figure 34: Parking Patio

Parking and Curbside Patio Combined: In some circumstances, a curbside patio can be combined with a parking patio. A combined 3.8 metre minimum is required. A partition is required outside the curb around the outer three sides of the parking patio, including the outer edges of the curbside portion of the patio.

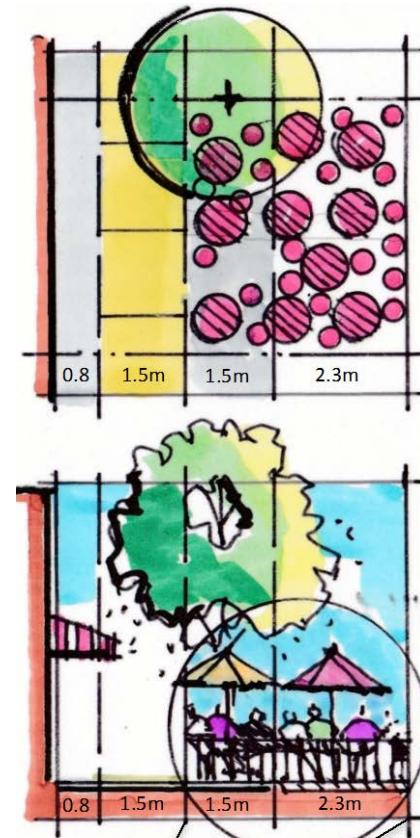


Figure 35: Parking and Curbside Patio



Recommendations

- An outdoor patio may occupy a maximum of one parallel parking stall (or portion thereof), two 90 degree stalls (or portion thereof), or a maximum of three angled stalls (or portion thereof). An outdoor patio shall not use both a parallel and an angled parking stall.
- The road side of the parking patio should have an appropriate partition – a temporary edge treatment that may include planters, railings, or cabling to protect patio patrons from moving traffic.
- The inside edge of the patio bordering the sidewalk may also be required to have a temporary partition. The patio partition should be a maximum height of 1.0m, as measured from the outdoor patio deck level.
- The partition should be visually permeable (see-through).
- Partitions should be cane-detectable (for the visually impaired) with a bottom rail 75mm above grade.
- Vertical members of the enclosure may be metal or wood and they should not exceed 50mm in dimension to allow adequate transparency through the enclosure.
- Planters are recommended on partitions or around the edges of the patio.
- Planters along the edge of the pedestrian clearway on the sidewalk, or along the street edge, should not impede pedestrian or vehicle traffic.
- Tables and chairs may be constructed of a range of materials including moulded plastic, resin wicker, decorative metal and finish grade wood with the assumption that all furniture is sturdy and safe for public use.



Photo: Existing parking patio on Baker Street with a solid non-permeable enclosure



Photo: Example of a frontage patio in Quebec City with a pedestrian clearway, a transparent enclosure that is approximately 1 metre high



- Colourful furniture is encouraged
- Maintenance and upkeep of the outdoor patios is the responsibility of the business owners.
- Sponsored umbrellas are not permitted.



Photo: Example of colourful stool seating along a building wall



Photo: Example of compact and colourful patio seating



Photo: Example of wicker patio furniture



Photo: Example of a permeable (transparent) enclosure in Nelson



Photo: Example of permeable Enclosure with ornate wrought iron railing



Photo: Example of a permeable enclosure



3.3.5 Awnings and Canopies

Awnings and canopies should provide continuous year-round weather protection along sidewalks in the downtown.

Awnings are usually temporary structures that are retractable or fixed and are made of a flexible materials such as canvas. Canopies are permanent structures made of inflexible materials such as wood, metal, or glass. Both awnings and canopies project from the exterior wall of a building.

Existing Conditions

Some of the existing awnings and canopies in the downtown are low or they extend too far into the sidewalk to allow adequate light to penetrate the sidewalk and store area. A number of them do not provide adequate weather protection for pedestrians and they are poorly maintained.

The alignment of awnings and canopies on the same façade or on buildings that are adjacent to one another is inconsistent, and the design elements and materials vary giving the appearance of an uncoordinated streetscape.

Recommendations

- Require a minimum projection from the building face for all weather protection of 1m and a minimum setback from the curb edge of 0.6m.
- Require a minimum clearance from the sidewalk of 2.5m.
- Awnings and canopies should respect historic architectural details.
- Require continuous weather protection for buildings on main streets in the downtown.
- Awnings and canopies should use colours that coordinate with the building façade. Primary colours should be limited to accents only (less than 10% of overall area).
- Retractable awnings are only permitted in temporary sitting areas.
- Retractable awnings will be mounted on steel or aluminum frames on a flat, angled slope.
- The area under the canopy should be enclosed to prevent birds from getting in, and to screen internal structural elements.
- Signage should be limited to awning drop area only (vertical section on front or side of the awning) (refer to Section 3.3.6 Signage for more information).



Photo: Example of the awnings in the 500 block of Baker Street. The alignment, design and materials vary from business to business.



- Contemporary design and building materials for canopies should include glass, metal and wood.
 - Wood materials shall only be used with the proviso that the beams are no wider than 0.3m. Additional support should be from above by cabling, and not from below.
- Down lighting within canopy structures is encouraged.
- Annual maintenance including cleaning and resurfacing should be required.
- The following are should not be permitted:
 - Arcades that extend over the entire sidewalk.
 - Awnings or canopies that overwhelm or dominate the façade (the awning or canopy is disproportionate in size to the façade concealing important architectural elements like windows).
 - Sheet vinyl materials.
 - Primary colours except for accents.



Photo: Example of a canopy with blade signage for each business



Photo: Example of quarter barrel-shaped fabric awnings



Photo: Example of a canopy supported by cables from above and including recessed pot lights



3.3.6 Signage

Coordinated and clear signage is important to an attractive and orderly downtown.

Existing Conditions

Currently, signage in the downtown lacks coordination leading to unnecessary clutter and ineffective communication. The height for fascia signs (wall signs) is inconsistent, as is the design for blade signs. Some store windows are dominated by signage that obstructs interior displays. Sandwich boards on the sidewalks create visual clutter and unsafe conditions for pedestrians. There is inconsistent lighting for signage and some signs are not lit at all.

Recommendations

- There should be a maximum of two signs per business plus window signs. Buildings may also have one directory sign.
- Fascia signs and hanging signs (blade signs) should be the primary signs for each business to communicate effectively with pedestrians and drivers.
- Create a consistent and continuous sign board area along the block. Upper or lower level signs should be removed.
- The use of sandwich board signs should be discontinued to eliminate clutter on sidewalks and associated safety hazards.
- Develop sign options for second storey businesses.

- Install secondary wayfinding markers on street corners that have blade signs for businesses or districts located off of Baker Street.
- Eliminate outdoor retail displays except on designated sidewalk sale days.
- Reduce the area dedicated to window signage to allow for greater transparency and opportunities for enhanced window displays.
- Allow awning signage on the drop area only.
- Provide exterior lighting in recessed entries.
- Encourage new signs with dimensional lettering that are lit from above.



Photo: Existing sign clutter on Baker Street



Photo: Existing sandwich board clutter on Baker Street



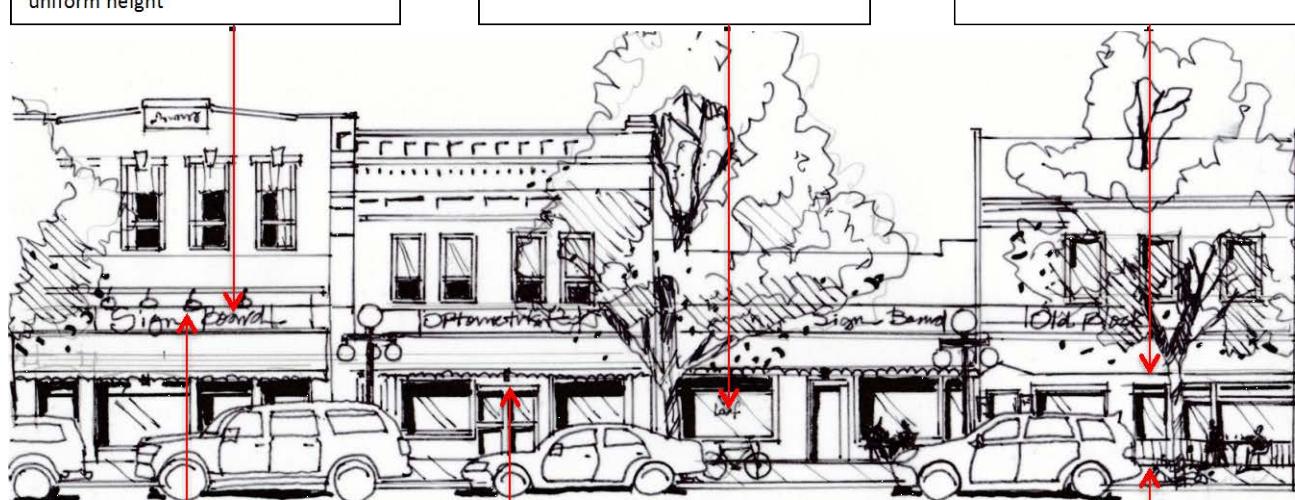
BAKER STREET
Royal Bank Block

BEFORE Signage
Improvements



BAKER STREET
Royal Bank Block

AFTER Signage
Improvements



**Create More Direct Front Lit Signage
Lighting**
To enhance overall visibility

**Locate Standard Individual Business
Directory Signs**
Under awning/canopy

**Eliminate Sandwich Board Signs on
the Streetscape**
Consolidate with blade signs on
wayfinding marker signs



3.3.6.1 Specific Signage Types

Recommendations

Fascia Signs

- Signs should be limited to a sign area above the first floor windows.
- Signs should have three dimensional lettering
- The maximum letter height and width is 300mm.
- Front illumination with billboard-type lighting fixtures is encouraged.
- Back-lit, plastic fascia signs are not permitted.

Awning Signs

- An awning drop skirt should have a maximum depth of 400mm.
- Lettering should be painted or applied vinyl, or incised lettering with an applied backing.
- There should be no rear lighting installed under awnings.
- There should be no signage or graphic material on any sloped, curved or vertical portion of an awning other than the drop are, as described above



Photos: Examples of different fascia signs - sourced from Google.com

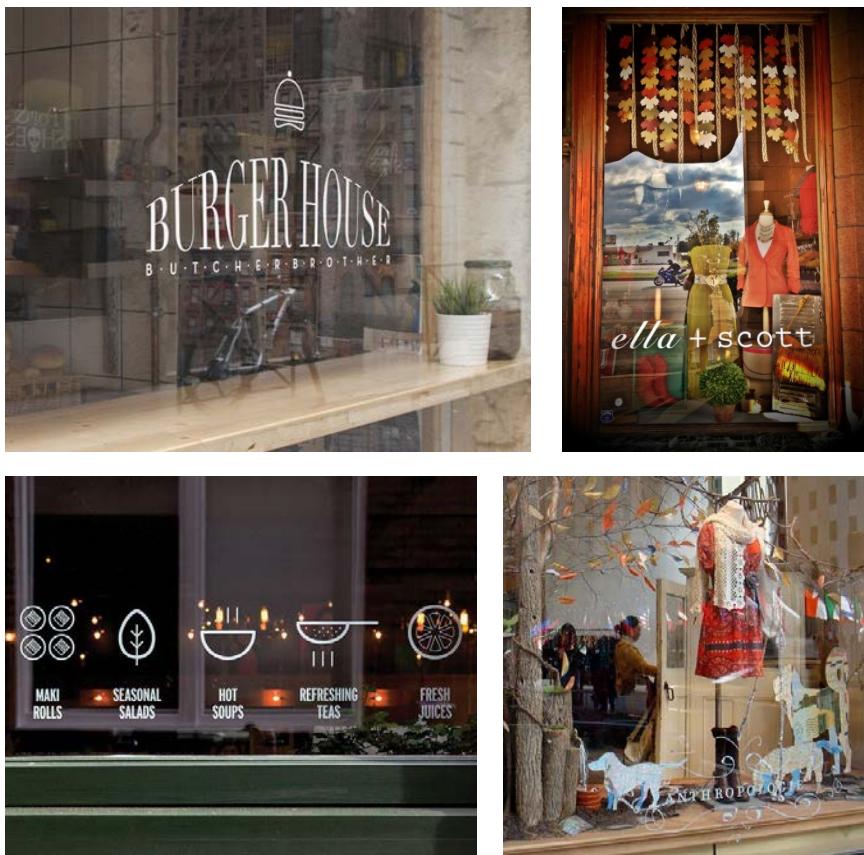


Photos: Examples of different awning drop signs - sourced from Google.com





Figure 40: Three Examples of Hanging/Blade Signage (Sourced from Google.com)



Photos: Examples of window signs - sourced from Google.com



Hanging/Blade Signs

- Signs should have a minimum clearance of 2.44m above the sidewalk.
- Signs should have a maximum area of 1m².

Window Signs

- Signs should have a maximum area of 0.5m² or 20% of the window area, whichever is more restrictive.
- Signs should not be constructed of paper, cardboard, plastic or fabrics. Cut-out vinyl letters or graphics, surface applied to inside of glazing are permitted.
- No back-lit signs, scrolling or flashing digital signs, displays, or product machines may be visible through store windows.
- Static neon signs are acceptable if they are installed inside the window.

Hanging Business Directory

- Signs should have minimum clearance of 2.44m² above the sidewalk.
- Signs should have a maximum area of 0.37m².
- Located at the front entrance of the building.

Materials

Exposed surfaces of signs may be constructed of any material with the exception of plastic, fiberglass, plywood or particleboard.

Colour

Signage colour should be coordinated with the materials and colours of the building facade and relate to the colours of the street block. The colour of the backing of a fascia sign should be consistent with the main colour of the building.

Height

Signs should not be more than 4m to 5m above the sidewalk. Signs located over a pedestrian right-of-way should have a minimum clearance of 2.44m above the sidewalk.

Lettering

The maximum permitted lettering size (height and width) on any sign should be 300mm. Symbols depicting the nature of the business are encouraged. Symbols should take up a maximum area of 0.5m².

Lighting

Front-lighting is encouraged to help illuminate signs. The use of rear lighting should be limited to: individual halo-lit lettering or symbols mounted on a solid, opaque background; or neon illuminated signs.

Storefront Transparency

Visibility into shops from the street should be maintained at all times. Any solid signage, advertising or blackout panels placed against the inside or outside of the storefront glazing should not be permitted.

Not Permitted

The following commercial sign types should not be permitted: pylon signs (free standing signs), back-lit sign boxes, billboards, revolving signs, pennants, bunting, flags (other than national, provincial, municipal flags), balloons or other gas filled inflatable devices, roof signs, changeable copy signs, sandwich boards or any other temporary signs, and digital signs.



3.3.6.2 Wayfinding

Existing Conditions

Wayfinding signage is important to direct visitors and tourists to different locations in the downtown. A significant part of the wayfinding signage in the downtown needs to be reoriented and made more visible. For example, the existing entry sign at the west end of Baker Street (in the east median) is not oriented south toward Highway 3A and motorists cannot see the sign from the highway.

Current wayfinding signs on Baker Street are located high on lamp standards and they are not readily visible to pedestrians or vehicles.

Recommendations

- A comprehensive wayfinding strategy should be completed for the downtown that includes a detailed analysis of the type of information to be included on wayfinding signs such as public parking locations, important civic buildings, public washrooms, and pedestrian connections to the waterfront and other parts of the City.
- **West Gateway:** Design a new primary gateway sign and associated landscaping for the Highway 3A gateway at Baker Street that speaks to the contemporary form and character of the Hall Street Plaza gateway markers.
- Integrate local materials of stone and timber in the design of the wayfinding sign.
- **North Gateway:** Integrate a vertical gateway marker on the west side of Ward Street at the south corner of Vernon Street.
- Create Secondary Wayfinding Markers with blade signs to advertise downtown businesses that are not located on Baker Street. This will eliminate the need for sandwich boards. The secondary wayfinding markers should be located in the proposed bulb-out areas on the north side of Baker Street.



Photo: Existing downtown gateway sign at Hwy 3A



Photo: Existing wayfinding sign mounted on a light pole

Primary Wayfinding

West Gateway: The redesigned primary gateway sign, as illustrated below, should integrate aspects of the Hall Street signage with local stone for the base of the structure. This sign should be visible from Highway 3A with clear lettering on a natural background. Shrubs should be planted at the base of the sign to help ground the sign.

North Gateway: As sidewalk space is limited in the north gateway area, a vertical marker sign is recommended at Vernon and Ward Streets on the southeast corner of the street.

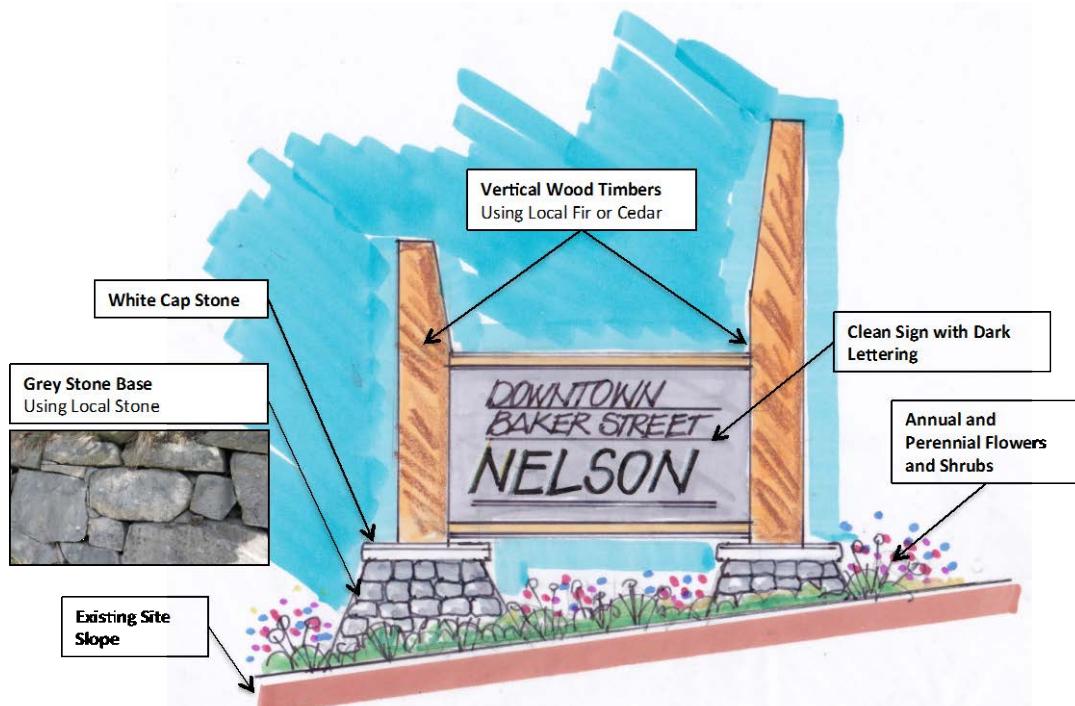


Figure 36: Primary west gateway sign at Baker Street and Highway 3A

*Dimensions and lighting to be determined

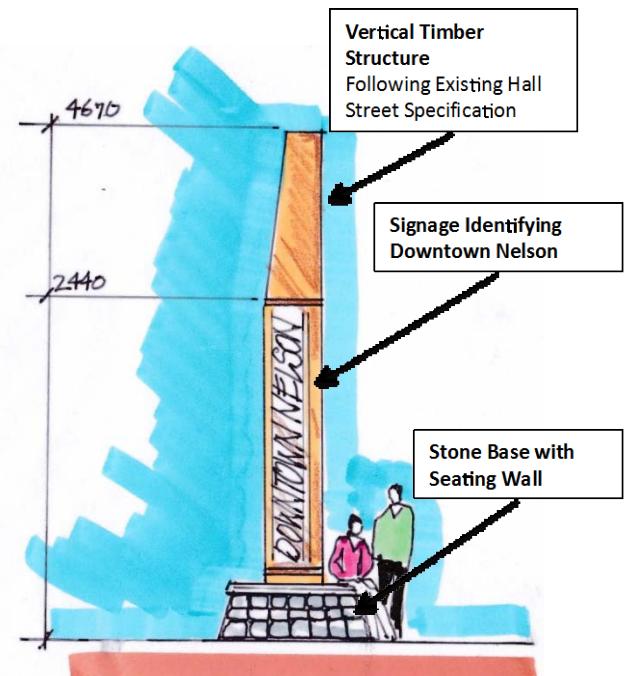


Figure 37: Primary north gateway sign at the intersection of Vernon and Ward Streets

Secondary Wayfinding

Secondary wayfinding markers should follow the basic form and materials of the Hall Street signage, but at a more pedestrian scale. The central vertical post should include a street sign, a block map for Baker Street businesses, and individual blade signage for businesses that are not on Baker Street, as well as key sites in the downtown.

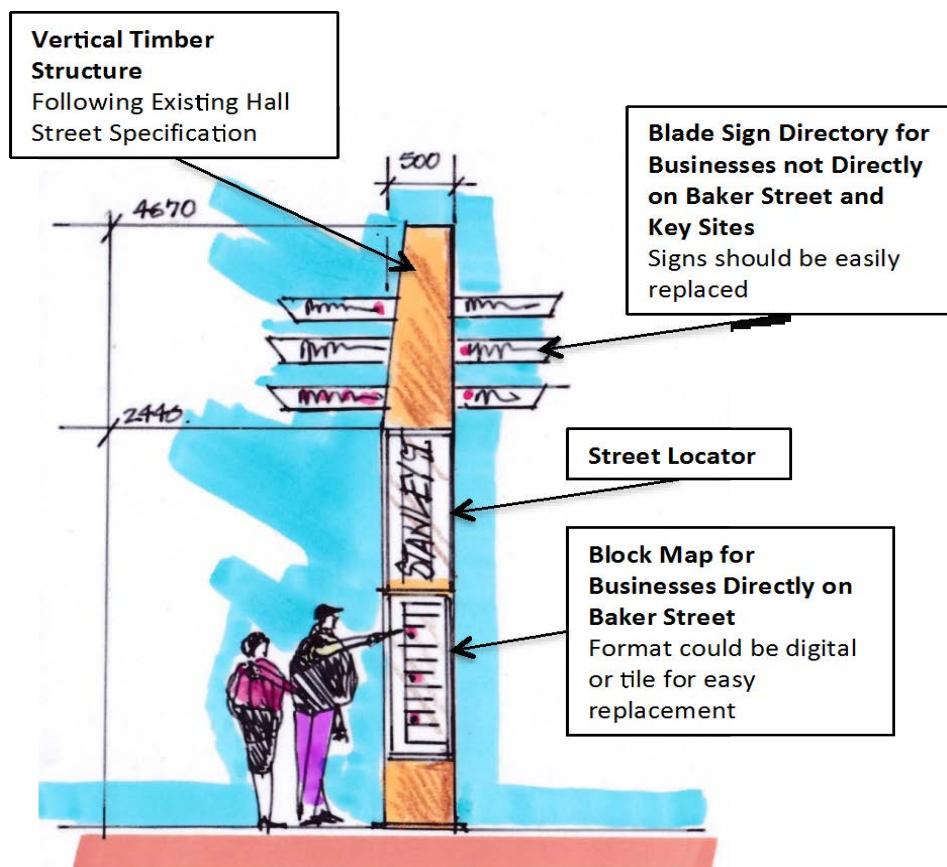


Figure 38: Secondary wayfinding marker on Baker Street



Additional Wayfinding

Additional wayfinding options include digital signboards in the public amenity areas along Baker Street, as well as at corners of Kootenay Street and Hall Street. These digital signboards could include a map of downtown businesses, key attractions, and notices to consolidate existing notification (poster) boards. Digital signboards can be changed on a weekly basis to stay current. They should be framed in timber to follow the application of the primary and secondary wayfinding signs.

Note: In lieu of digital signboards, the existing notice boards should remain in place and be refurbished.

Wayfinding

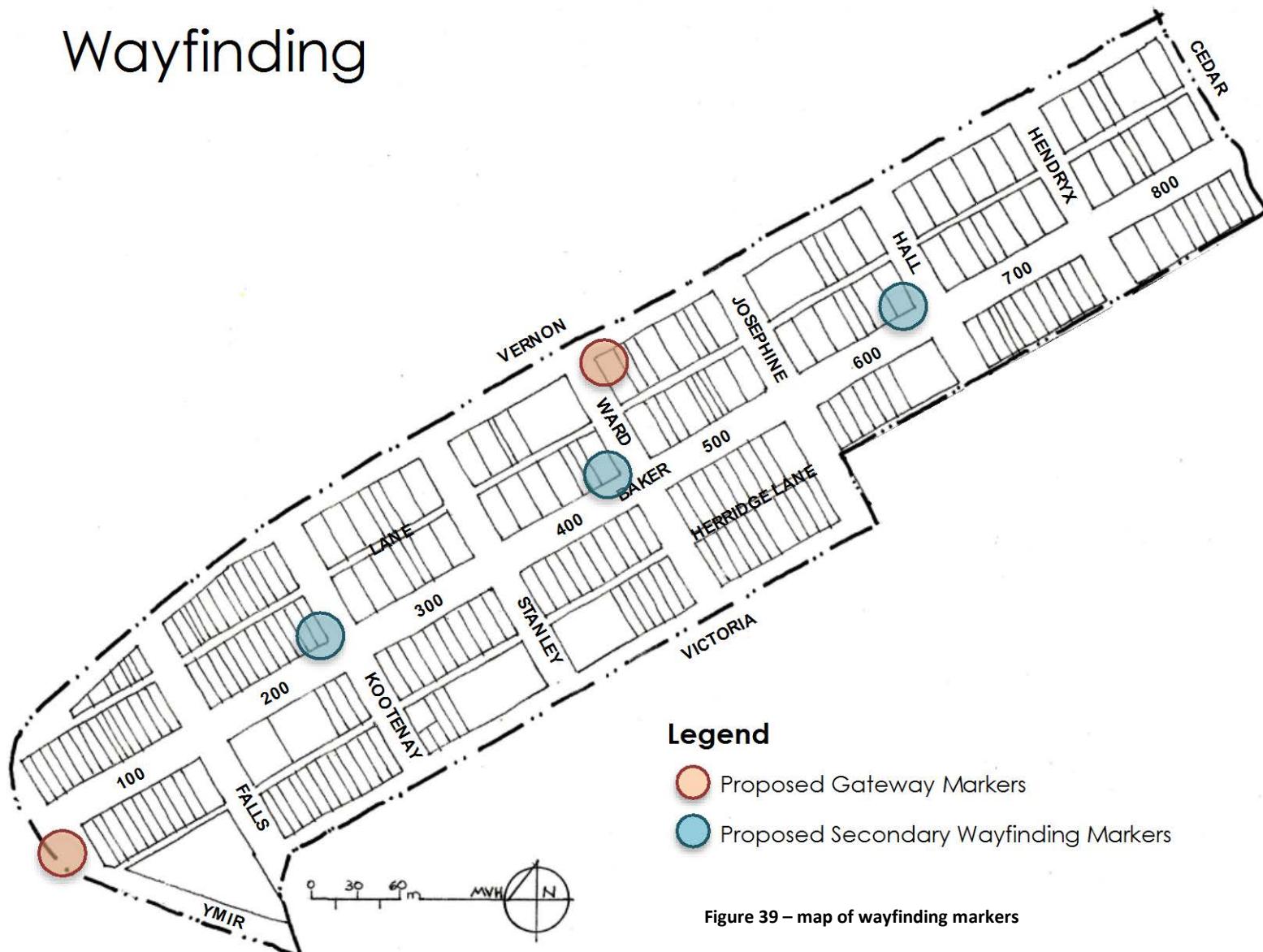


Figure 39 – map of wayfinding markers



3.3.7 Lighting

Existing Conditions

It has been determined that the existing historic multi-globe street lights in the downtown need to be replaced. A recent study indicated that the light poles are failing due to age and associated structural deterioration. The recommendation is that all the historic light poles -- an estimated 100 street lights, be replaced immediately.

The existing contemporary lighting on Vernon Street is working well with no apparent issues. In time, as these light standards age it is recommended that they be replaced with one of the contemporary light standard styles identified below. Section 3.3.7.1 outlines options for replacement light standards on Baker, Victoria and Vernon Streets, as well as the connector streets in the Downtown.

Different kinds of lighting can bring new life to the downtown after dark. The goal is to implement lighting that creates a pleasing nighttime environment, while at the same time meets safety and security objectives.



Photos: Existing Globe Lighting in the Downtown

Recommendations

- New street lighting should be dark sky compliant and energy efficient. The recommended LED lighting is more energy efficient and offers a historic acorn style that is dark sky compliant.
- Lane lighting should work for pedestrians and cyclists. Due to space constraints, lights should be mounted on buildings and existing light poles. Care should be taken to ensure that lights do not conflict with large delivery trucks that use the lanes.
- Overhead catenary lighting is recommended for the Baker and Ward Streets intersection. It will accent the town centre and provide options for seasonal holiday lighting. Overhead catenary lighting can also be used in the two public amenity areas in the 400 block of Baker Street. Additional seasonal lighting options can be custom designed to adorn trees and light standards.

Photos: Existing lighting on Baker and Victoria Streets



3.3.7.1 Street Lighting

Recommendations

Selection of streetlights for the downtown should follow these criteria:

- Minimize light pollution
- Energy efficiency (LED)
- Colour selection and fixture design to fit the specific area
- Prioritize Canadian companies

Light standard options:

Heritage Style (Baker Street and Ward Street)

- i. **Globe style** lighting aligns with the heritage character of the current light standards on Baker Street. These globes can be configured to match the existing light standards and offer an option for replacement without changing the style of lighting. The drawback to replacing the existing standard with a similar globe style of lighting is that it causes light pollution.
- ii. **Acorn style** lighting provides a capped lighting option with a heritage style standard.

Transitional Style (Victoria Street and Cross Streets)

- i. **Globe style** lighting can coordinate with the heritage character of the light standards on Baker Street. The drawback the globe style of lighting is that it causes light pollution.
- ii. **Domus style** is recommended for Victoria Street and all the connector streets in the downtown. This style of light coordinates with the heritage style on Baker Street and it is energy efficient and dark sky compliant.

Contemporary Style (Vernon Street)

- i. **Custom style** reinforces the existing light standard design on Vernon Street. It is dark sky compliant and matches the light standards installed on Hall Street.
- ii. **Domus style** lighting is consistent with the transitional style, but with a more modern bracket and pole. This fixture is also dark sky compliant.



Photo: Capped lighting option for Baker Street



Examples

Heritage Style



Option #1: Lumenarea
Victoria VICB80



Option #2: Lumenarea
Prestige 3690

Transitional Style



Option #1: Philips Lumec
New Westminster Series



Option #2: Philips Lumec
Domus 55 Series

Contemporary Style

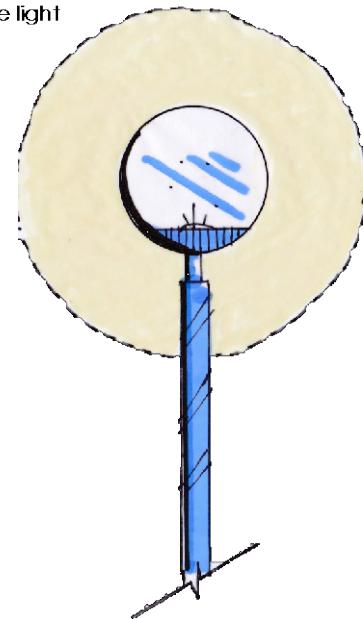


Option #1: Custom Existing
City Spec



Option #2: Philips Lumec
Domus 55 Series

Globe lighting option with
no direction to the light



Capped lighting option
directing light downwards

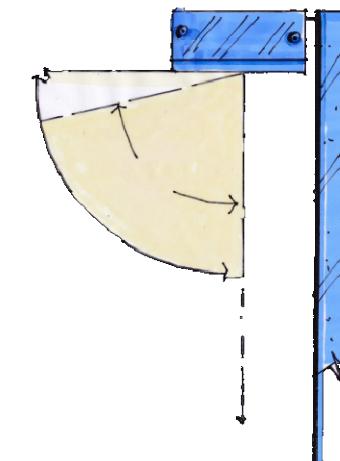


Figure 40: Street light styles



3.3.7.2 Holiday Lighting Recommendations

- Overhead catenary lighting should be installed at the intersection of Ward and Baker Streets. It should be linked across the intersection in a grid and anchored by four tall vertical poles at each corner of the intersection. The vertical posts can be extensions to the light standards. The light bulbs may be changed during the holiday season to red and green. Additional holiday lighting may be added to the lampposts and trees.
- A detailed holiday lighting plan should be prepared for the downtown to address specific questions related to the density and quality of lighting within the blocks and on street trees. The plan could also look what holiday decorations are appropriate.



Photo: Holiday decoration on light poles (Google.com, 2016)



Photo: Cross street banner style holiday lighting with center globe lights (Google.com, 2016)



Photo: Holiday wreath attached to a light pole



3.3.7.3 Building Lighting

Fixtures mounted on buildings or on canopies over the sidewalk can provide directed light that enhances the pedestrian realm and can accentuate signage, public art and architectural features. This lighting also improves safety. Building lighting should be directed to pedestrian and cyclist areas, and it should not cause glare.

Lighting the Lanes

The two lanes on the north and south sides of Baker Street provide alternative walking and cycling routes. There are a limited number of light fixtures in the lanes, and most of the light is oriented toward vehicles (the fixtures are up high and the light is less directed).

Recommendation

- Building mounted lighting is appropriate in the lanes due to the limited public right-of-way. It is recommended that wall mounted fixtures be installed approximately 3m above grade. The light should be directed down for a pedestrian/cyclist orientation and the light beam should stretch at least half way across the lane. Initial installation should focus on the blocks immediately adjacent to the downtown core.

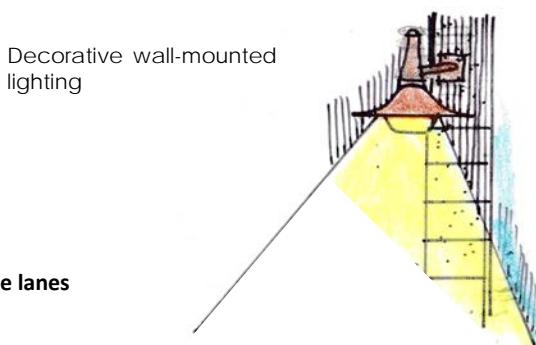
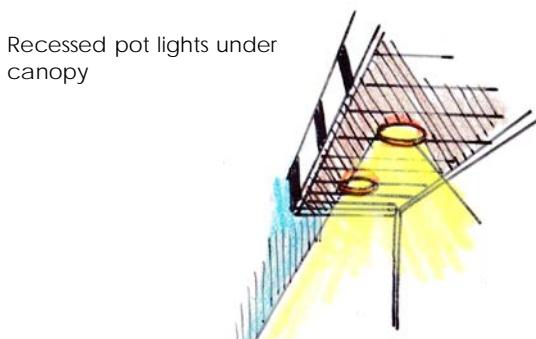
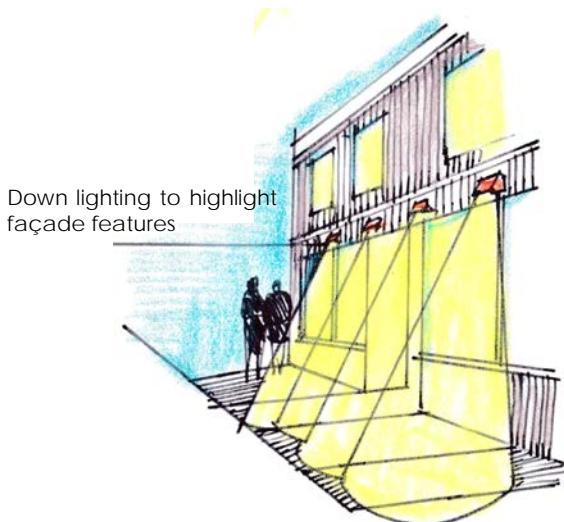


Figure 41: Lighting options for the lanes



3.3.8 Street Furniture

Discussion of street furniture design elements in the downtown is based on the following street structure and defining characteristics that are described in detail in Section 2:

- **Heritage Style** - Baker Street and Ward Street
- **Transitional Style** - Victoria and connecting Streets
- **Contemporary Style** - Vernon Street

Heritage Application – Baker Street and Ward Street

The proposed heritage application of street furniture is respectful of the extensive heritage character of both Baker Street and Ward Street. Black is the overall heritage application colour. This is consistent with the existing colour palette.

Transitional Application – Victoria Street and all other connecting streets

Victoria Street is the southern transitional street between the uphill neighbourhoods and the downtown. The transitional application represents a more subtle application of the heritage style. Black is the transitional application colour.

Contemporary Application – Vernon Street

Vernon Street has a more contemporary design application. Silver is the primary design colour. The proposed elements are similar to the ones installed on Hall Street.

Location and Placement

Street furniture should be located along the curb in sidewalk zone 3 (refer to section 3.3.1.1 Sidewalk Zones).

Selection Criteria for Street Furniture

- Respect the heritage character of the downtown, as well as the character of the individual streets.
- Choose elements that emphasize Nelson's heritage and surrounding natural environment (Schedule H OCP, 2013).
- Use sustainable and recycled materials (Schedule H OCP, 2013) including metal, wood, concrete, plastic or recycled composite.
- Work with local artists for custom furnishings in bulb-outs.
- Where possible, choose Canadian companies first for street furniture to be cost effective and allow for parts to be easily replaced when necessary.
- Consider weather, durability and maintenance due to Nelson's climate.
- Choose materials that are vandal resistant.
- Choose LED lighting and consider the reduction of light pollution where possible.
- Coordinate furniture materials, colours and character.
- Select for comfort and seasonal durability.
- Select ready-made and readily available furniture.
- Consider full cost, including the purchase price, shipping, installation and maintenance.



3.3.8.1 Benches

There are a variety of bench styles in the downtown and the benches are constructed with a variety of materials. Maintenance levels vary and some benches are in poor condition. The new benches on Hall Street have a contemporary application with wood and metal, but the wood has greyed and it is in need of upkeep.

Recommendations

- Provide one bench option corresponding with the specific street application styles - heritage, contemporary and transitional.
- Create more formal and informal groupings of moveable chairs to increase seating opportunities in the downtown.
 - Use a variety of colours to add accent and interest.
 - Explore options to lock up chairs in the evenings with security chains or having business owners become stewards.
- Benches and movable seating should be free of advertising.
- Orient benches towards pedestrian traffic along the curb in sidewalk zone 3 to provide a clear walkway for pedestrians.
- Locate benches in close proximity to planting areas.
- Ensure benches have back support and arm rests for comfort.
- Choose benches with an option for skateboard deterrents, including guards, material direction, and arm rests.
- Surface-mount benches to the sidewalk.
- Re-oil/stain the benches on Hall Street to remove grey tones and refresh their appearance.
- Explore opportunities for sponsorship where businesses and community members can adopt benches that have recognition plaques on them.
- Implement a schedule for regular replacement of benches in the downtown.



Photos: Existing different styles of benches in the Downtown



Examples

Heritage Style



Option #1: Canadian CAB-820

Transitional Style



Option #1: Maglin HBSP

Contemporary Style



Option #1: Maglin MLB 1200



Option #2: Wishbone Classic



Option #2: Victor and Stanley CM324



Option #2: Equiparc EP1980 with LED



Option #3: Forms & Surfaces Knight Bench

Movable Seating

Option #1: Maglin Battery Collection



Option #2: Maglin FRC1700



Figure 42: Benches and moveable seating options



3.3.8.2 Waste/Recycling Bins

Existing Conditions

There are a variety of waste receptacles in the downtown and they lack coordination. A limited number of the waste receptacles have recycling options. Maintenance is an issue and some receptacles have been vandalized or are in disrepair. The wood finishes on the receptacles on Hall Street are new, but have weathered poorly and need to be re-stained/oiled.

Recommendations

- Provide one waste/recycling receptacle option corresponding with the specific street application styles - heritage, contemporary and transitional.
- Orient receptacles towards pedestrian traffic along the curb in sidewalk zone 3 to provide a clear walkway for pedestrians.
- Locate receptacles with adequate distance from seating areas to avoid nuisance odors.
- Surface-mount waste receptacles to the sidewalk.
- Re-oil/stain waste receptacles on Hall Street to remove grey tones and to refresh their appearance.
- Implement a schedule for regular replacement of waste/recycling receptacles in the downtown.



Photos: Variety of waste/
recycling receptacles in the
downtown



Examples

Heritage Style



Option #1: Maglin MRC 202



Option #2: Canaan CAR-182 Recycling Station

Transitional Style



Option #1: Equiparc EC3700-RU-Double



Option #2: Canaan CAR-182 Recycling Station

Contemporary Style



Option #1: Equiparc EP 3520-RU-Double



Option #2: Maglin LXRC1503-48-MS



Option #3: Forms & Surfaces Apex

Figure 43: Waste and recycling container options



3.3.8.3 Bike Racks

Existing Conditions

There are a variety of bike racks in the downtown and it is not clear which racks are to be used for bike parking. Sometimes parking metres are used as bike racks. Many of the racks are in poor repair and there are a limited number of racks that can accommodate more than two bikes at one time. Overall, there is a shortage of designated bike parking downtown and there are no covered bike parking facilities.



Photos: Existing bike racks in the downtown

Recommendations

- Provide additional bike racks in the downtown that are easy to access. This will promote active transportation and bicycle commuting.
- Provide one bike rack option for each street or cross-street that corresponds with the three different applications styles – heritage, contemporary and transitional.
- Provide a minimum of 0.6m in width, 1.8m in length, and 2m in clearance height from grade for each bicycle parking space.
- Surface mount bike racks to the sidewalk.
- Ensure that short term bike parking is convenient, well lit, and visible.
- Create an overall bike parking strategy to provide locations and adequate spacing for both long- and short-term bike parking.

- Maintain a minimum of 1.75m between bike parking, and street furniture or vehicles.
- Provide bike racks that support the bike in at least two places and allow the user to lock their bike with a U-style lock.
- Choose bike racks with high quality materials and fabrication standards that are resistant to cutting, rusting, bending, and deformation. Use materials that are sustainable and recycled where possible including metal, wood, plastic or recycled composite.
- Choose bike racks that require minimum maintenance.
- Orient bike racks parallel to the curb in sidewalk zone 3 to provide a clear walkway for pedestrians.
- There should be a minimum of two bike racks per block up to a maximum of eight in high traffic areas.

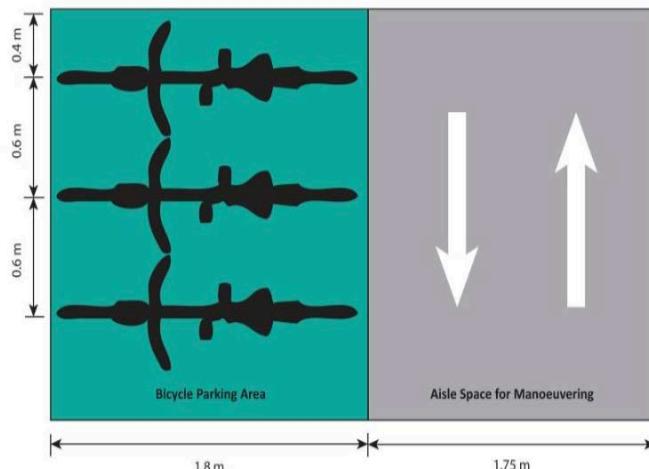


Figure 44: Bike parking design requirements



Examples

Heritage Style



Option #1: Wishbone
Classic 2 Space Bike Rack



Option #2: Dero
Custom Hoop Rack

Transitional Style



Option #1: Anova
Arch Powder Coated Bike Rack



Option #2: Wishbone
Classic 2 Space Bike Rack

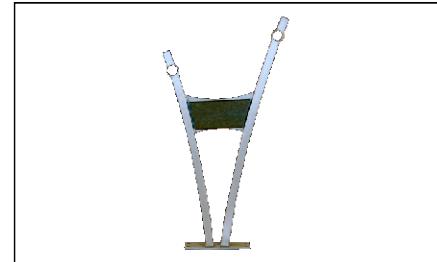
Contemporary Style



Option #1: Dero
Bike Bike Rack



Option #2: Equiparc
EP59 with LED



Option #3: Wishbone
Turisno 2 Space

Figure 45: Bike rack options



3.3.8.4 Water Fountains

Existing Conditions

There are currently no water fountains in the downtown, but a location a new water fountain and bottle filling station is planned for the intersection of Hall and Baker Streets.

Recommendations

- Water stations are recommended at: 1) the new transit station on Victoria Street; 2) the intersection of Baker and Hall Streets; and 3) the intersection of Ward and Vernon Streets.
- Water fountains should correspond with the appropriate applications style for the location – heritage, contemporary and transitional.
- Locate water fountains in areas with high pedestrian traffic.
- Water fountains should be suitable for use in a winter climate and an urban street environment. Water fountains may include a bottle refill station.
- Use materials that are sustainable and recycled where possible including metal and/or concrete.
- Locate water fountains along the curb in sidewalk zone 3 to provide a clear walkway for pedestrians.

Examples

Heritage Style



Option #1: Haws Model 3511FR



Option #2: Haws 3377FR With Bottle Attachment

Transitional Style



Option #1: Haws Model 3511FR



Option #2: Haws 3377FR With Bottle Attachment

Contemporary Style



Option #1: Urban Forms + Surfaces Apollo 280



Option #2: Haws 3377FR With Bottle Attachment

Figure 46: Water fountain options



3.3.8.5 Bollards

Existing Conditions

Light standards and parking metres currently act as bollards in the downtown. New contemporary custom bollards have been placed on Hall Street with a rusted metal application.

Recommendations

- Provide one bollard option for each street or cross-street that corresponds with the appropriate style applications type -- heritage, contemporary and transitional.
- Strategically place bollards in locations where there is the potential for conflict between pedestrians and vehicles, including at intersections, between parking and pedestrian areas, and at bulb-out locations.
- Provide chains in specific areas to direct pedestrians away from roads.
- Place bollards along the curb in sidewalk zone 3 to provide a clear walkway for pedestrians.
- Surface-mount bollards to the sidewalk.
- Provide bollards with lighting in key areas where there is high pedestrian traffic including amenity areas on Baker Street.
- Consider movable bollard options for seasonal parking patios on Baker Street.



Photos: Different bollard types on Hall Street



Examples

Heritage Style



Option #1: Sternberg Lighting Decatur 4501LED



Option #2: Maglin MTB 650

Transitional Style



Option #1: Maglin MTB650

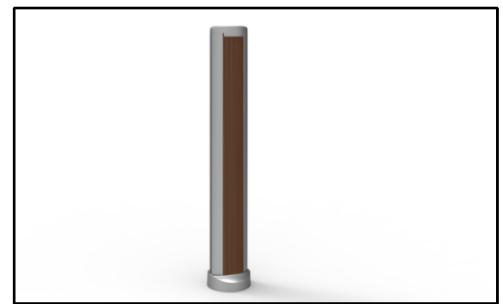


Option #2: Sternberg Lighting Parkside 4555LED

Contemporary Style



Option #1: Custom Existing Bollard



Option #2: Maglin SCTB1600

Figure 47: Bollard options



3.3.8.6 Paving

A range of different paving treatments are identified including cast concrete, asphalt and modular products. The materials and patterning are one of the most highly visible aspects of open space systems. Higher quality materials, banding, and colours serve to emphasize areas of prominence while utilitarian materials such as cast concrete are best utilized to cleanly organize the utility zone.

The following palette of paving materials is appropriate for Baker Street. Any other products or designs that are proposed in the future should be compatible in materials and character. Stamped concrete or stamped asphalt paving are not recommended. Cast concrete score lines should be sawcut not trowelled.

Granite Sett Unit Pavers

Granite sett pavers should be incorporated in the premium and intermediate level sidewalk. Opportunities for adding concrete banding at regular increments are encouraged. Granite setts should vary in size and feature a flamed finish— from 4"x4" setts in the main sidewalk and 12"x24" setts at the corners of intersections.

Concrete Unit Pavers

A variety of concrete or brick unit pavers should be incorporated in the premium and standard street levels. Patterns and colours may vary, though grey tones are preferred as the dominant theme with accents of warm tones.

Concrete

Cast concrete should feature a broom finish with sawcut score lines. Score pattern should vary. Stamped concrete paving is not recommended.

Special Stone Paving at Bulb-outs

Stone insets may be used at the street corners as feature paving to highlight the names of the streets at intersections. In these instances, either Granite or Basalt is encouraged. Name insets should follow those used for Hall Street following the dimensions outlined on the following page.



Figure 48 – Paving Options



NOTES:

1. NAME PLATE TO BE SET FLUSH AND TRUE TO CONCRETE SIDEWALK AND ALL EDGES.
2. NAME BLOCKS TO BE LOCATED AS PER PLAN. LAYOUT TO BE CONFIRMED IN FIELD PRIOR TO CONSTRUCTION.
3. FONT TO BE HELVETICA, CENTRED WITHIN PLATE, 150mm HGT.
4. LETTERING TO BE ETCHED ONTO METAL PLATE - MIN DEPTH 3mm.
5. ANCHOR PINS MOUNTED TO UNDERSIDE OF PLATE, WET SET INTO SIDEWALK CONCRETE.
6. QUANTITIES:
4 x HALL
4 x BAKER
2 x HERRIDGE

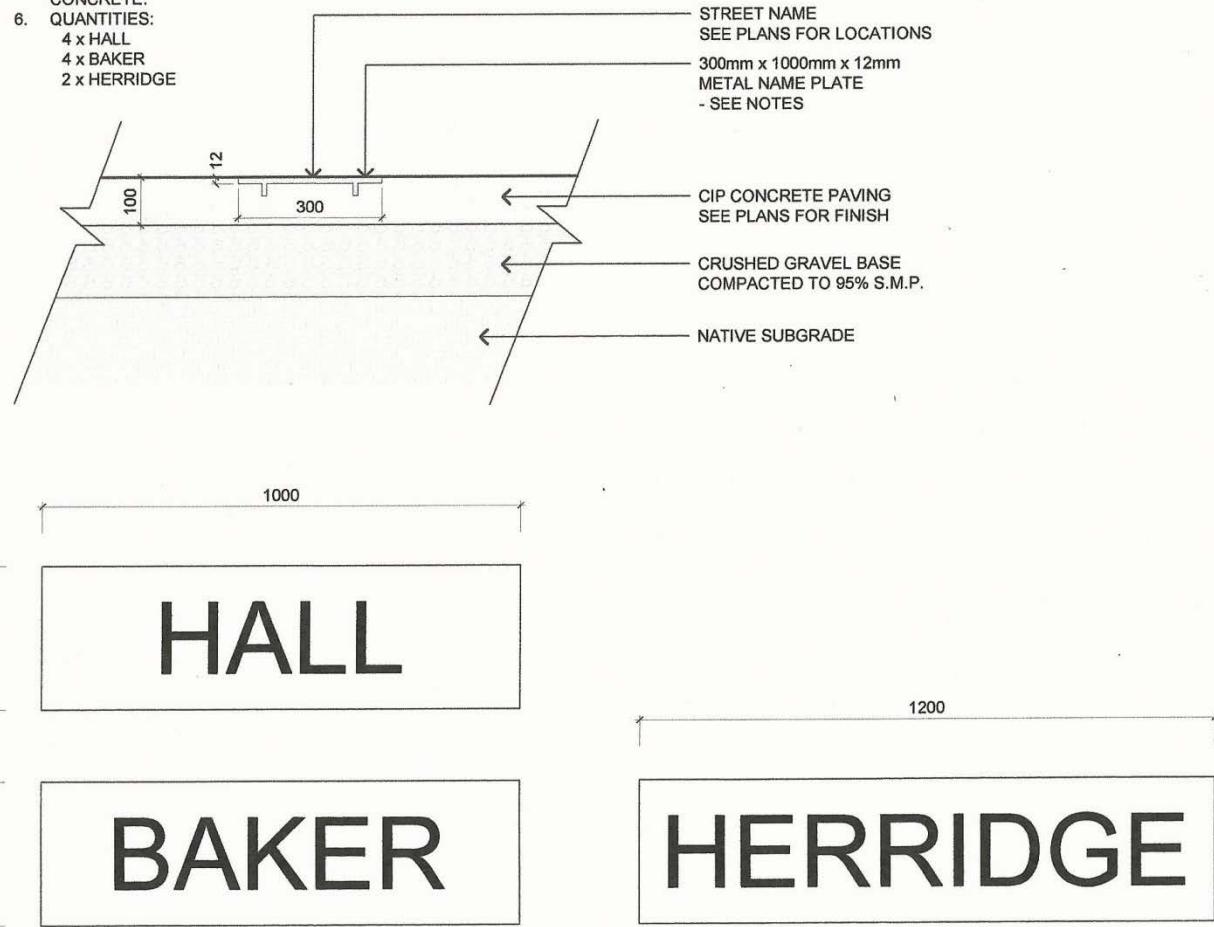


Figure 49: Hall Street design plan for street name insets (MMM Group, 2015)



3.3.9 Landscaping

3.3.9.1 Urban Trees

Trees provide a number of benefits as described in detail in Section 2.9 – Urban Ecology. Mature trees help define the downtown and they reflect the heritage of Nelson. The first priority for tree management in the downtown is to retain healthy trees that are not damaging infrastructure, or preventing street reconstruction efforts.

When trees need to be removed due to their age, poor health, or the need for sidewalk and street repairs, it is important that they are replaced with the right trees in the right places. Special consideration should be given to the fact that urban growing conditions are harsh (e.g. confined planting areas) and that trees in Nelson are subject to varying seasonal weather conditions.

Though not a formal plan, the City of Nelson five year Tree Management Plan is a guideline and summary of what Public Works would like to accomplish regarding urban trees. The goal of the plan is to follow a 2:1 tree replacement ratio. Public Works and Nelson Hydro are working together to ease the cost of pruning and removals at sites where the trees are impacting overhead hydro lines. The City only plants tree species that are suitable for the site conditions and that meet size requirements.

Public Works' tree replacement program has specific criteria for when to remove a tree including that the trees are diseased, in poor health, damaged or are not the appropriate species for downtown areas. Some of the trees in the downtown area have become too large for the planting sites and are damaging business awnings and storefronts. The other issue is with tree roots impacting underground infrastructure, roads and sidewalks.



Recommendations

- The Colorado blue spruce and cherry trees located on the edges of the public amenity areas in the 400 block of Baker Street were assessed by Public Works in 2016 and it was determined that all four trees are in relatively good health considering the challenging growing conditions. They are between thirty and thirty-five years old, which is significant given that the average life expectancy of trees planted in confined space is fifteen years. Given their age and the conditions under which they are growing, it is recommended that these trees be considered for replacement when the amenity areas are rebuilt.

If these trees are replaced, new trees should be selected based on the selection criteria below.

If the stone wall planters are removed, replacement trees should be planted at grade. If a mid-block crossing is installed, care should be taken to ensure that plantings do not interfere with visibility to the street.

- There should be a diversity of trees in the downtown to protect against potential species-specific diseases. It is recommended that new tree planting areas be a minimum of 1.5m by 1.5m with tree grates (unless these trees are planted in the Vernon Street central median). Planting areas should be filled with engineered structural soil to allow water and soil nutrient retention. It is recommended that a "Silva Cell System" or similar product be combined with structural soils and stormwater infrastructure (pipe connections) to allow for improved stormwater detention and management along downtown streets.

NOTES:

1. TREE GRATE TO FINISH FLUSH WITH ADJACENT SURFACES.
2. DEPTH OF ROOTBALL TO MATCH ORIGINAL ROOTBALL DEPTH ESTABLISHED IN THE NURSERY. ANY TREES PLANTED TOO LOW SHALL BE RAISED AT CONTRACTOR'S EXPENSE.
3. ROOT BARRIER TO BE DEEPROOT UB 18-2 AS SUPPLIED BY DEEPROOT CANADA Tel: 800.561.3883. INSTALL AS PER MANUFACTURER'S INSTRUCTIONS.
4. COORDINATE CIP OPENING WITH GRATE DIMENSIONS PRIOR TO POUR.

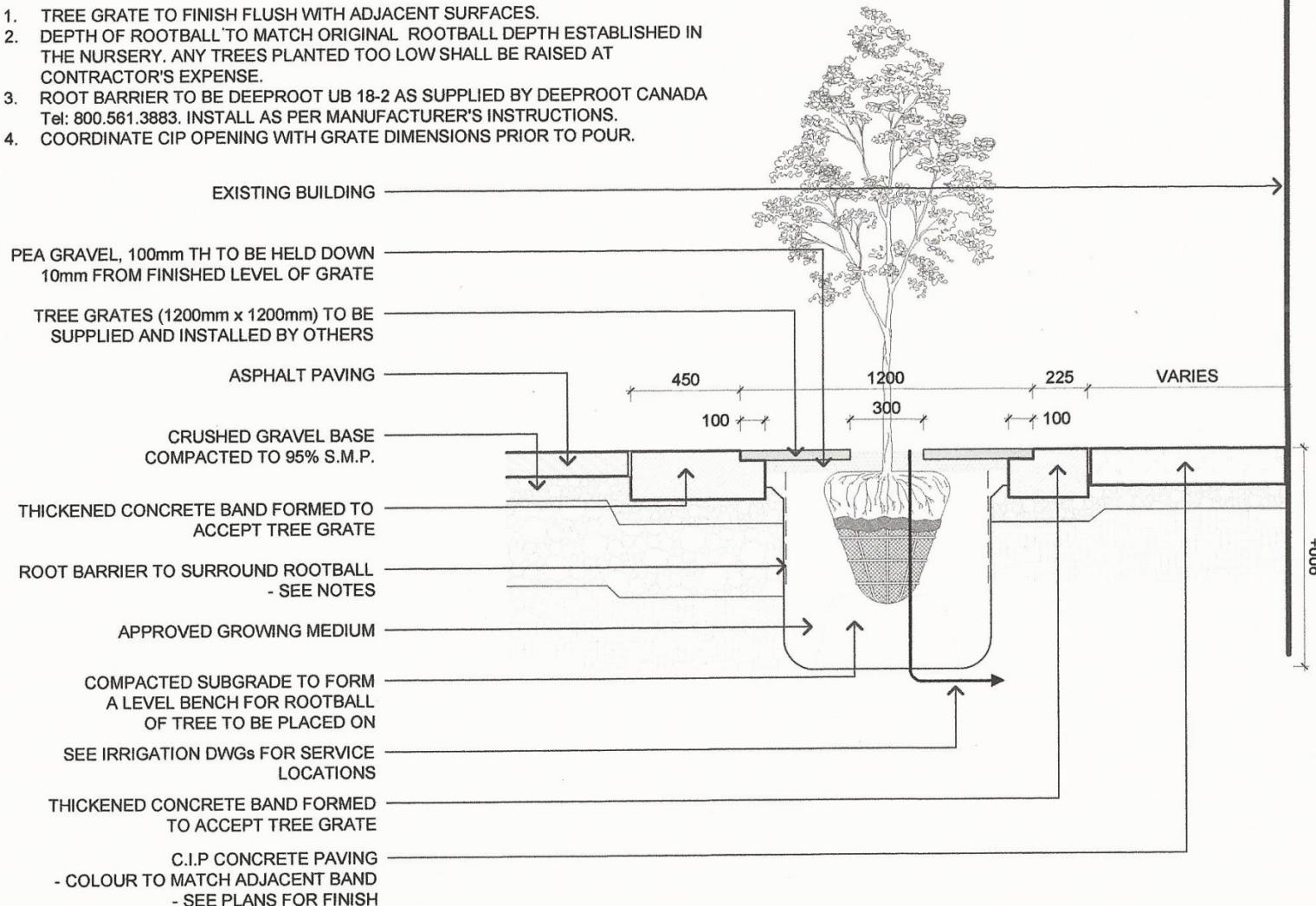


Figure 50: Hall Street design plan for tree grate finish design with DeepRoot barrier system to be augmented by a Silva Cell-Type System (MMM Group, 2015)

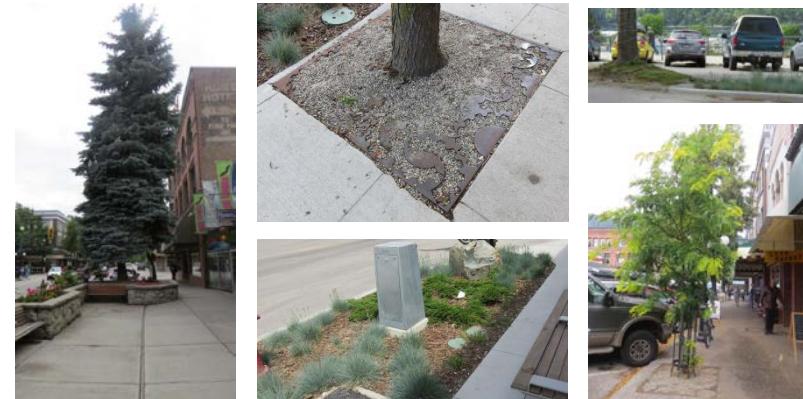


Recommendations

- More trees are required in the 500 block of Baker Street and it is recommended that 3 to 5 trees be planted per side, per block depending on underground services and utilities. There are tree planting limitations on the north side of Vernon Street between Ward and Josephine, as well as on the west side of Ward Street between Baker and Victoria Streets, and the north side of Baker Street at the corner of Ward Street.
- The acorns from the oak trees on Vernon Street create problems for maintenance staff and they are unsafe for pedestrians and cyclists. When the medians on Vernon Street are redesigned the oak trees should be replaced with more appropriate tree species.
- The recommended distance between trees is 10m.
- Street trees should be located in the sidewalk edge zone (see Section 3.3.1.1 for sidewalk zones) and in bulb-outs.

Selection criteria for replacement trees include:

- Choosing native species.
- Local availability.
- Size at maturity.
- Expected root growth.
- The density and visibility of the tree canopy.
- Seasonal colour.
- Water requirements.
- Maintenance requirements.
- Potential nuisances including susceptibility to disease and undesirable insects.



Photos: Urban trees and plantings in the downtown



In accordance with the Tree Management Plan, a tree replacement program will ensure that aging and declining trees are replaced over time.

Year	2014	2015	2016	2017	2018
Removals	3	3	1	5	
Plantings	6	6	6	7	4

Figure 51: Downtown tree replacement from 2014 to 2018

Downtown Planting Palette

Trees

Armstrong
Red Maple



Maidenhair



Star Magnolia



Honey Locust



Shrubs

Green Carpet Juniper

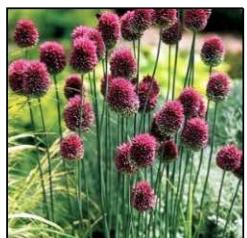


Apricot Surprise Azalea



Perennials

Drumstick Allium



Autumn Joy Sedum



Grasses/Sedges

Japanese Blood Grass



Berkeley Sedge



Rosy Sedge



Elijah Blue Fescue



Blue Oat Grass



Shenandoah Switch Grass



Hamelin Fountain Grass



Blue Little Bluestem



Korean Feather Reed Grass



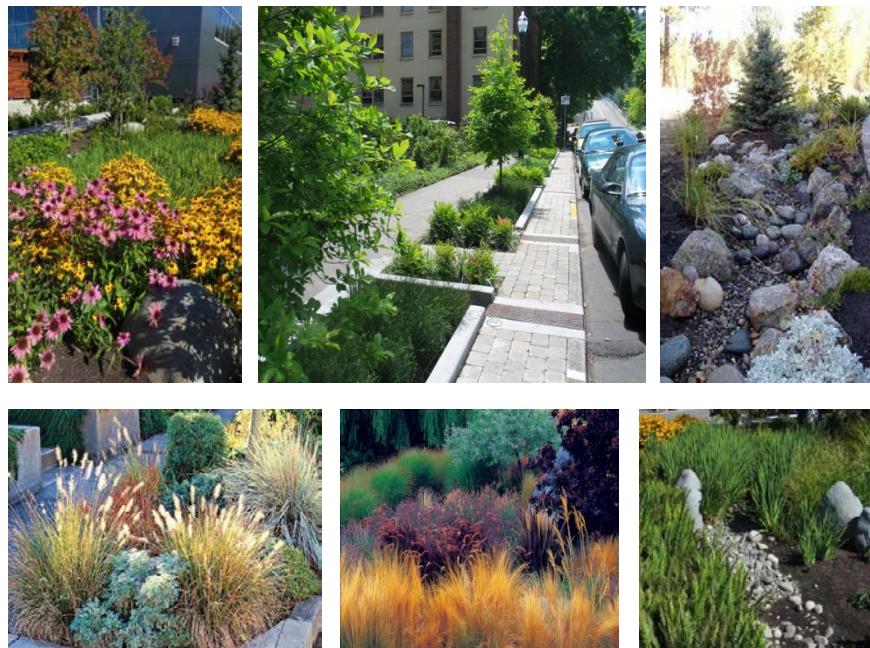
Figure 52: Recommended planting palette for downtown that follows the planting plan for Hall Street (illustrative images sourced from google.com)



3.3.9.2 Planting Beds

Recommendations

The proposed bulb-outs at intersections will provide additional opportunities for planting beds in the downtown. Examples of this are the new planting areas in the bulb-outs on Hall Street. Existing planting beds along Vernon Street and along the residential edges of the downtown are encouraged. These are further described in Section 2.9 Urban Ecology.



Selection criteria for plants to go in the planting beds:

- Use of native species.
- Use of low water consumers.
- Long-term maintenance requirements.
- Seasonal colour.
- Density of plantings to achieve a short term and long-term effect.
- Seasonal hardiness of the species.

In addition, these planting beds can offer the potential for stormwater management to absorb runoff and recharge the soil. The stormwater can be channeled through the beds with appropriate absorption areas.

Figure 53: Examples of rain garden designs that include directed drainage areas, as well as detention areas and colourful plantings.



3.3.9.3 Container Plantings & Hanging Baskets

Recommendations

Container plantings and hanging baskets that add seasonal colour, texture, and fragrance to the streetscape are recommended. The City's hanging baskets that are attached to light standards add seasonal colour and vibrancy to the street edge. Seasonal flowers and plantings can extend the life of hanging baskets and container plantings. Container plantings are also an opportunity for businesses to express their unique styles and to define their entrances. Planters in the street edge zone (see Section 3.3.1.1 for sidewalk zones) should be standardized. Planters in the bulb-out areas.

Businesses can provide unique planters in accordance with the specifications outlined below. The following standards should be considered for the container plantings to ensure that the containers do not clutter the sidewalk or create a safety hazards:

Container Planter Standards:

- Location limited to the 0.8m building edge zone on the sidewalk unless associated with an outdoor patio (see Sections 3.3.1.1 and 3.3.4).
- Maximum 1m in height.
- Materials and colours to blend with the facade of the building. Accent colours are encouraged.

Examples



Figure 54: Planter options



Figure 55: Examples of planters and hanging baskets



3.4.0 Public Art

Public art refers to original works of art that have been planned and executed for temporary or permanent installation in a location that is accessible and clearly visible to the public. Public art includes sculptures, murals, creative displays, and aesthetic enhancements to City infrastructure including street furniture and utility boxes. It is an important cultural resource in the City of Nelson and is guided by the Art in Public Places (APP) Policy and the work of the Cultural Development Committee (CDC). The Policy directs the Art in Public Places program and provides a framework for developing public art in Nelson.

In accordance with the policy, CDC members are encouraged to consider a number of guiding principles in the selection of public art including:

- Style and nature.
- Quality of the work.
- Local involvement.
- Media.
- Elements and design.
- Life span.
- Permanence.
- Diversity.
- Risk assessment.
- Acquisition.

Recommendations

- The review and selection of public art should be a multi-party initiative that includes the Heritage Working Group (HWG) and City staff.

- With the extensive and rich inventory of heritage buildings and landscapes of significance on the Heritage Register, including designated buildings, the City's public realm should be carefully planned so to not undermine the importance of the heritage backdrop and rich architecture.
- To further enhance Nelson's unique artistic and cultural signature, it is recommended that downtown street furniture and related street elements for bulb-outs be designed and fabricated locally, where possible. The tree grates in the recent Hall Street improvements were custom-designed using local recycled industrial metal to create an artistic flare, reflecting part of the industrial economy of the region.



Photo: Temporary public art piece in the amenity area of the 500 block of Baker Street



3.4.0.1 Sidewalks

Recommendations

Building on the Art in Public Places Policy, this strategy recommends that the placement of public art on sidewalks be primarily focused in the public amenity areas and the bulb-outs.

The following criteria should be used to review public art location and appropriateness:

- The public art is placed in the street edge sidewalk zone or on the inside building edge zone (see Section 3.3.1.1 Sidewalk Zones for further details).
- The public art piece will not obstruct a clear pedestrian zone of 1.5 metres.
- The public art is of an appropriate size, fabrication and content that fit the space and context so that no unnecessary hazards are created by its placement.
- Labeling of public art is consistent and each label includes information on the origin and meaning of the art piece.
- Public art should be placed on well-crafted support structures, the design and construction of which should be which is consistent for all pieces.
- The City may make exceptions to the selection criteria above if the public art piece is of extraordinary merit or is a temporary installation that deserves special treatment and placement

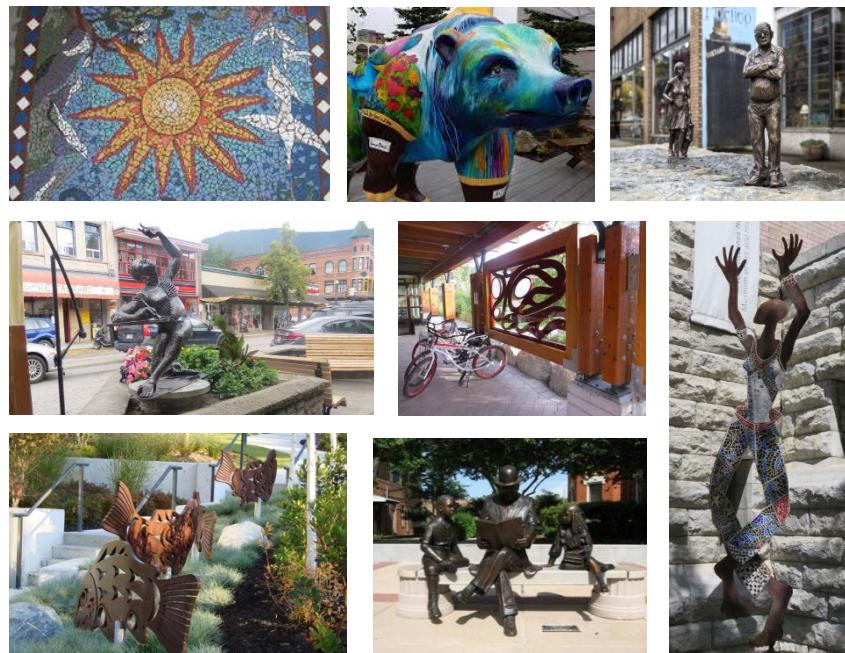


Photo: Examples of public art



3.4.0.2 Murals

Recommendations

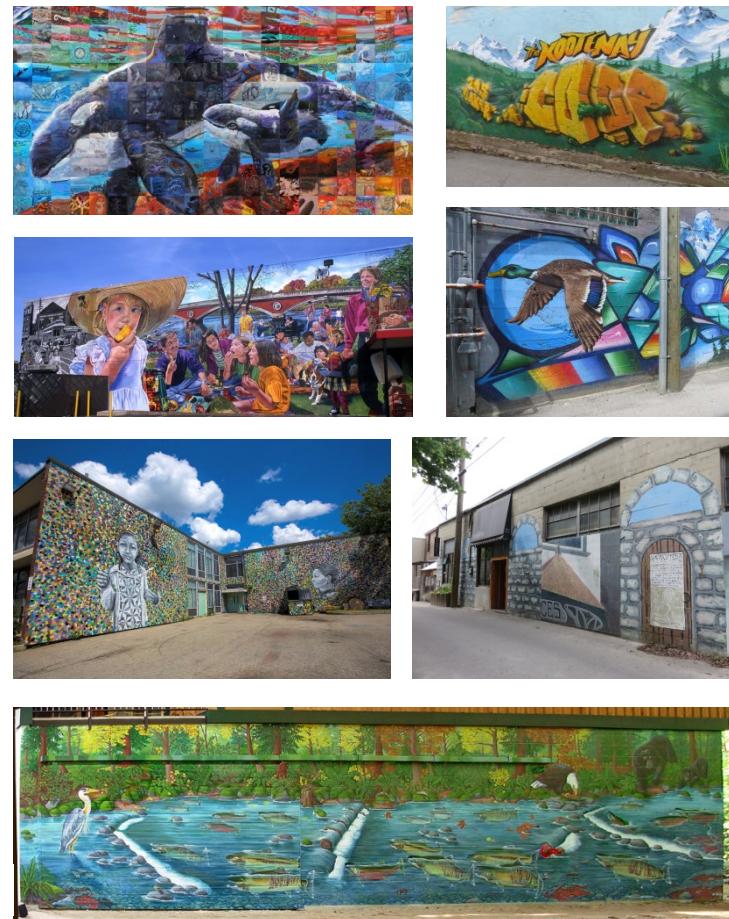
A mural is defined as any large-scale artwork, painting, or mosaic located on the exterior wall of a building or on a ground surface. Murals should be primarily located on the walls in the back lanes to activate and enliven these spaces. Proposed murals should concentrate in the central area of the downtown first in close proximity to Baker and Ward Streets. The procedure and approval of murals should follow the City's murals policy.

The City's *"A Practical Guide to Creating a Mural"* outlines steps to follow to create a mural. The following criteria are also recommended to help guide the selection, location and content of murals in the downtown:

- The content shall meet the City's artistic standards and it must be seen as contributing to the art and culture.
- Local stories and history are favourable, as are contemporary works.
- Murals should have rich graphics, and they should not rely on lengthy text or quotations.
- The content shall not include advertisements, business names or explicit depictions of products sold on the premises.
- Profanity, hateful language or depiction of violence or weaponry are not permitted (murals related to historical war commemoration will be considered).
- The signature of the artist is permitted, but at an appropriate scale with no graffiti tag.
- The mural should be visible from the street or lane
- The materials used in the murals should be durable and include a non-toxic anti-graffiti coating so that any vandalism can be more easily removed.

Photos: Examples of wall murals

- Where lighting is included, the lighting must be directed at the mural and minimize any glare or spillage.



Murals

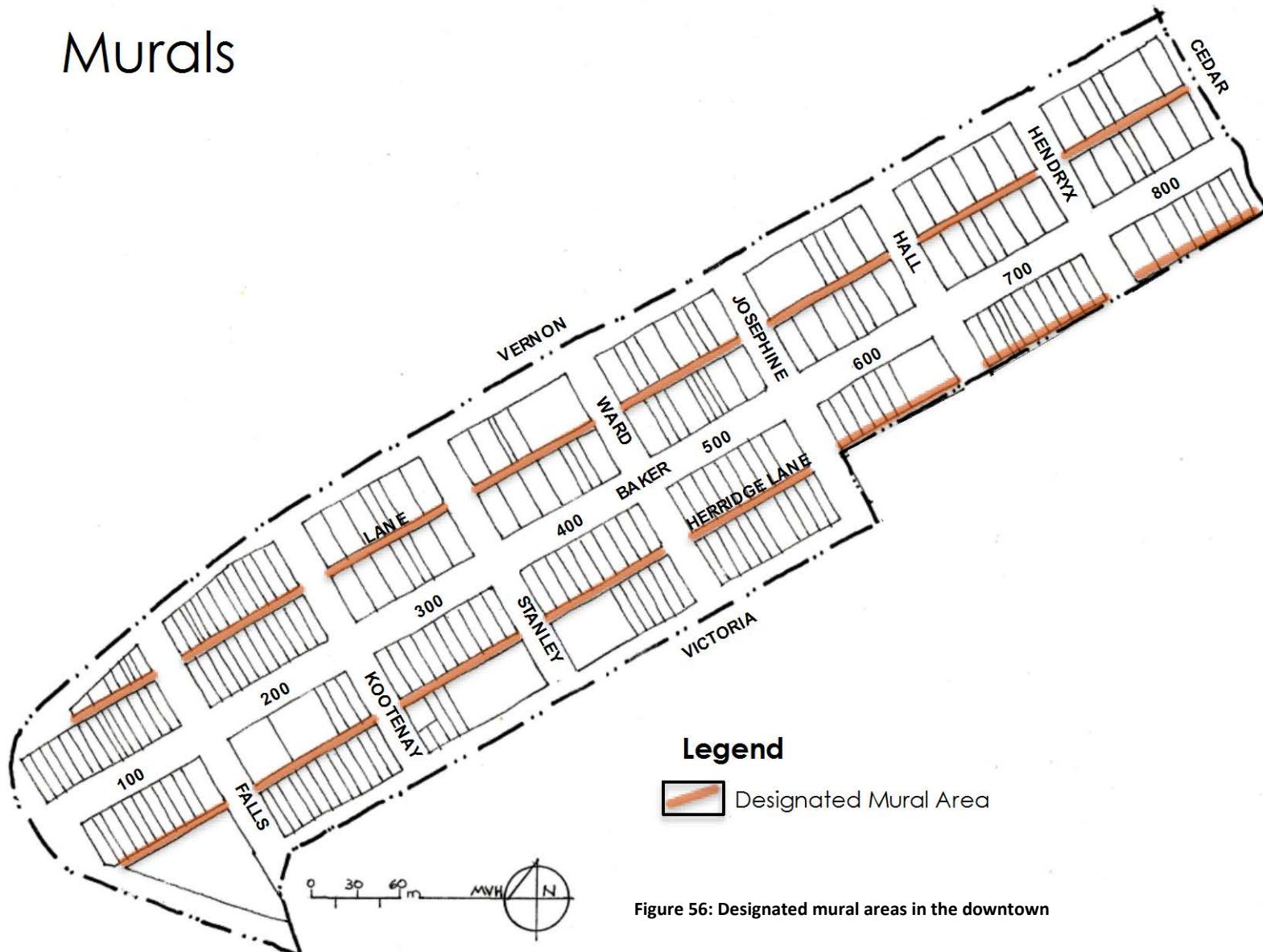


Figure 56: Designated mural areas in the downtown



3.4.0.3 Utility Boxes

Utility boxes are another way to incorporate public art in the downtown. They can be an effective graffiti reduction measure and can help the community to further express their identity.

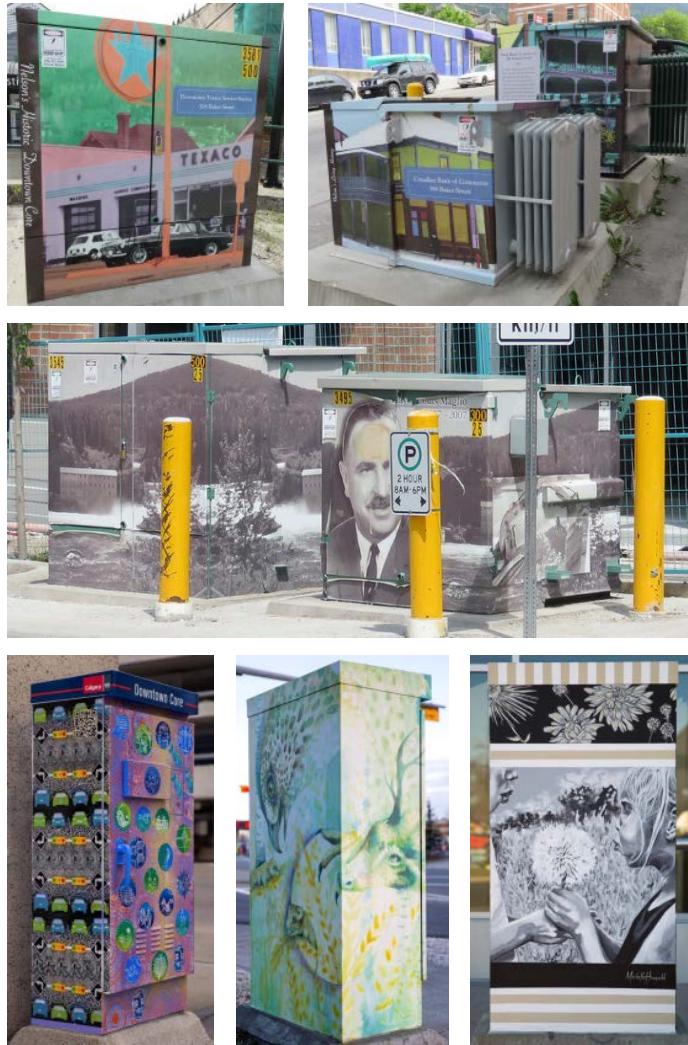
Recommendations

To further develop the existing utility box program it is recommended that the City:

- Develop a list of priority utility boxes in the downtown.
- Create a process for selection of local artists.
- Involve the community in the design development process to incorporate the community's specific identity.

It is recommended that the criteria for murals outlined above also apply to utility boxes with the following additions:

- The public art will pay particular attention to subdued paint or media applications that fit with the colours of the surrounding buildings and natural context of the City.
- Themes should pay particular attention to Nelson's history and culture, and its natural context. The physical surroundings of the boxes' locations should be considered as well.
- Designs should not promote specific downtown businesses.
- Designs should limit the amount of negative space to deter vandalism and graffiti.
- Extensions or attachment to the boxes as part of the art are not permitted.
- Utility boxes are functioning pieces of equipment. Over time, they will need repair or replacement. Artists should be aware that their artwork could be damaged or removed at any time.
- Maintenance will be the responsibility of the City of Nelson.



Photos: Examples of painted utility boxes.



3.4.1 Washrooms

Washrooms have been requested by the community to support a vibrant and active downtown. The City is installing a new washroom at the corner of Hall Street and Baker Street.

Recommendations

It is recommended that the City consider adding one or possibly two new washrooms at the following locations:

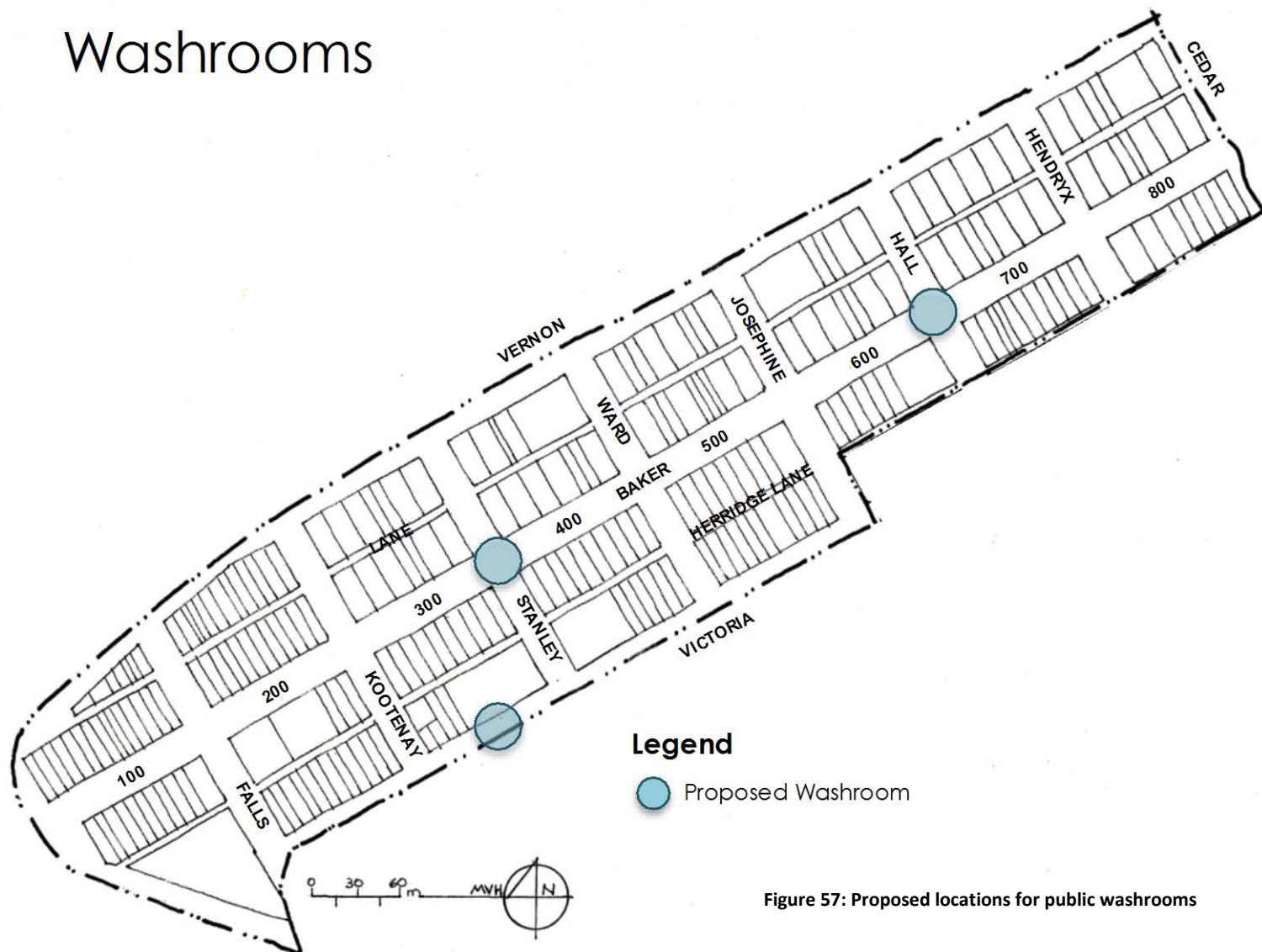
1. At the new transit station on Victoria Street, should the existing transit station be re-located.
2. On the corner of Baker Street and Stanley Street.



Photos: Examples of public washrooms in Portland, Auckland, Calgary, and Boston. Imaged sourced from Google.



Washrooms



3.4.2 Electric Vehicle and Car Share Parking

Car shares and electric vehicles help to reduce GHG emissions, which is consistent with the direction of the *Path to 2040 Sustainability Strategy and Active Transportation Plan*.

Recommendations

To further support electric vehicles and the local car share program, specific priority locations for parking in the downtown core are recommended:

1. **Baker Street:** At least 2 of each of car-share and electric vehicle parking spaces.
2. **Victoria Street:** One of each on Victoria Street in the 400 and 500 blocks between Stanley and Josephine Streets
3. **Vernon Street:** One of each on Vernon Street in the 500 and 600 blocks between Ward and Hall Streets.



Photo: Examples electric vehicle parking and a charger, as well as signage for electric vehicles and car shares - sourced from Google.



4



IMPLEMENTATION



4.1 INTRODUCTION

The implementation plan outlined in this section is the roadmap for action on the recommendations of the Downtown Urban Design Strategy. Some of the actions outlined in this section can be completed immediately, while others may take longer, as they will require additional resources and more time for review.

The implementation plan outlines **what** should be done, **when** it should be done, by **whom** and **with what** resources. It sets priorities for action and creates a framework to consider what is required in terms of resources. It also ensures that the City can take a coordinated approach to implementation, outlining how staff and elected officials can work with downtown businesses and landowners, and community organizations to take action on specific tasks. Ultimately, the City will determine the priority and timing of these recommended actions through its budgeting process.

The implementation plan is built on four pillars (4Cs) to ensure improvements are concentrated, connected, complementary and collaborative:

Concentrate

- Nelson's strong commercial core is the priority area for increased retail, office uses, and community and social services.

Connect

- The improvements to the three principal streets—Baker, Victoria and Vernon Streets, will facilitate better connections between retail areas, public buildings, local services and public amenities. They will also continue to enhance the downtown as the destination for residents, as well as regional day-visitors and tourists.
- Nelson's topography and its street grid enhancements should visually and physically connect the flatter retail areas with the waterfront, as well as commercial services on Front Street, industrial areas on Lakeside Drive and in Railtown, and the adjacent residential neighbourhoods.
- Improved signage and wayfinding will increase connections between businesses on the streets and in the lanes.



Complement

- Further development of a design aesthetic which speaks to the unique historic and contemporary features of Nelson will provide a framework to link old and new public amenities in the downtown area.
- Recommendations for Baker, Victoria and Vernon Streets, as well as the lanes should recognize recent improvements on Hall Street.
- Proposed street furniture and plant materials will be used to visually define and complement each of three main street zones within the downtown area.

Collaborate

- Stewardship of the public realm in Downtown Nelson is a collaborative venture—the City, property owners, merchants, business organizations will continue to find collaborative ways to support the implementation of civic improvements and urban design.
- A collaborative approach between the public, private and non-profit sectors can help to secure the financial resources, organizational leadership and volunteer support needed to advance community projects.
- Maintenance and management of the public realm is a community endeavour—these are the places and spaces enjoyed by everyone—and engaging community organizations in a leadership role, through programs such as “Adopt-a-Block” or direct sponsorship of benches, moveable chairs or planters will strengthen collaboration between and among organizations.
- Collaboration with the Nelson Police Department is necessary to ensure that City bylaws are enforced.

Priorities and Sequencing of Implementation

It is recommended that the City direct implementation in accordance with the following priorities and sequencing:

1) Update existing and creating new, policy and regulations:

Short-term actions should include updating existing policy and regulatory documents for signage, awnings and canopies, and outdoor patios.

The City should also explore different options and tools to address key issues in the downtown that were identified through the public consultation. Though these issues cannot be solved through urban design, they warrant consideration as part of the larger conversation about long term planning for the downtown. The issues discuss include:

- The street population (Nelson’s street culture);
- Panhandling, begging and loitering;
- Smoking in public;
- Parking management;
- Traffic congestion

2) Focus on Baker Street:

Above and below ground infrastructure on Baker Street needs to be replaced within the next five years and this should be an early priority for implementation of the Downtown Urban Design Strategy. Improvements on Vernon and Victoria Streets, as well as in the lanes, and on connector streets can move forward in tandem as opportunities for funding and adjoining new developments.



Reconstruction of Baker Street could proceed in one of two ways:

- i. Begin at Highway 3A to allow for efficiency and incremental replacement of underground and above ground infrastructure from west to east, two to three blocks at a time. There is less activity at the west end of Baker Street, which may allow the City to 'test' the construction sequencing and work with business owners in order to develop the optimum construction methods before reconstruction work begins in the core.
 - ii. Start reconstruction at the intersection of Ward and Baker Streets, and work outward in all directions. This would create a significant and immediate change to the streetscape, which would signal the City's commitment to implement changes in the downtown that support pedestrians.
- 3) **Make pedestrians the priority** building on existing city policy and plans, prioritizing pedestrians in the downtown should be the basis for all decision making. Any future design work should consider pedestrian.



4.2 26 PRINCIPAL ACTIONS

The following list summarizes the principal actions for improvement of Nelson's downtown public realm. These actions correspond with the 10 big moves identified in Section 3:

- 1. Rebuild Baker Street in stages.** Above and below ground infrastructure on Baker Street will need to be replaced within the next five years. To minimize impacts on businesses, a staged approach is recommended from west to east over the eight blocks. The City of Nelson should work with local businesses to develop a detailed construction plan so that businesses are least impacted by construction.
- 2. Replace Baker Street trees.** As part of the reconstruction of the street, existing should be assessed and replaced, where necessary. This will ensure the right trees are replanted in the right places and it will help to improve sidewalk function. It will also provide additional stormwater management capacity and reduce physical impacts on sidewalks and to infrastructure.
- 3. Incorporate stormwater management and root barriers in tree planting.** When the City replaces trees downtown, sub-surface water detention, continuous planting areas, structural soils, and root barriers should be integrated into the construction design for improved stormwater management and optimal tree growth.
- 4. Expand public amenity areas.** The existing public amenity areas on Baker Street should be redesigned and, where feasible new bulb-outs should be added at street corners to expand the amount of public space in the downtown. Considerations of traffic flow and necessary right-hand turn lanes may limit these bulb-outs in certain cases.
- 5. Replace the street lights.** The existing street lights require replacement throughout the downtown. The street light poles are deteriorating and they are difficult to maintain. The globe-style bulbs result in light spilling into adjacent residential dwellings. It is recommended that the lights be replaced in the short-term with similar lights that are more energy efficient and dark sky compliant.
- 6. Create the Nelson “Town Square” at the intersection of Baker and Ward Streets.** The intersection at Baker and Ward Streets is the centre of Nelson and it likely has the highest pedestrian volumes in the downtown. The new design transforms this intersection into a slightly elevated square with special paving, overhead lighting, a “scramble” traffic light configuration, and sidewalk furnishings. The option exists to close the town square to vehicular traffic, along with the 400 and 500 blocks of Baker Street for special events and festivals.
- 7. Develop parking patios in the 400 Block of Baker Street.** As part of street reconstruction, elevate the parallel parking spaces in the 400 block to the height of the sidewalks and add removable bollards so that these areas can become outdoor patios during summer months, and parking spaces the rest of the year. The bollards will separate the outdoor patios from the traffic.



8. **Apply the street furniture recommendations.** This study recommends a specific application of street furniture elements on Baker, Victoria, and Vernon Streets, as well as the connector streets in the downtown. The Baker Street reconstruction is an opportunity to replace the furniture. Other streets may replace street furniture with redevelopment or eventual street reconstruction.
9. **Focus lane Improvements.** Future expansion of businesses, along with improvements such as murals, pedestrian lighting and lane pathway painting should be focused on the core area first to develop a critical mass of projects and associated improvements in the lanes.
10. **Rewrite the sign bylaw.** The City will be rewriting the sign bylaw. It is recommended that the new regulations follow the direction provided in this study. Illustrations should be added to support the text and provide visual suggestions of what to do and what not to do. The new sign bylaw will coordinate signage in the downtown and reduce the visual clutter on the street.
11. **Revise the mural policy.** The mural policy should be amended to ensure that murals are regulated under specific content and visual design guidelines, and to clarify locations in the downtown where they are permitted.
12. **Create a three zone sidewalk area.** Establish a sidewalk with three zones to ensure safe and functional sidewalks in the downtown. Zone 1 is a pedestrian clearway zone that is a minimum of 1.5m wide. Zone 2 is a building edge zone that is a minimum of 0.8m wide. Zone 3 is a street edge zone that should be a minimum of 1.5m wide.
13. **Amend the guidelines and regulations for awnings and canopies.** It is recommended that the regulations for awnings and canopies be updated to reflect the specific recommendations in this strategy.
14. **Develop a bylaw for Outdoor Cafes.** It is recommended that a bylaw be created for outdoor cafes (referred to as outdoor patios in this strategy). The regulations in the bylaw should follow the recommendations made in this strategy.
15. **Eliminate sandwich boards and limit retail displays on sidewalks.** The recommendations to remove sandwich boards and to limit outdoor retail displays may be a sensitive issue for some community members, but it is necessary in order to create a safe and uncluttered sidewalk environment, especially for residents and visitors with mobility issues. To ensure that off-Baker Street business still have profile, additional secondary wayfinding markers are recommended to be installed on north side of Baker Street. Sidewalk displays will be permitted on designated sidewalk sale days.
16. **Redevelop the west Baker Street entrance.** Design and install new signage at the west Baker Street entrance at Highway 3A. Coordinate with the Ministry of Transportation and Infrastructure to install a signalized intersection at the intersection of Highway 3A and Baker Street that includes improved pedestrian-activated crosswalks.
17. **Improve the north gateway at Vernon and Ward Streets.** It is recommended that the City install a vertical gateway marker on the south corner of Ward and Vernon Streets to frame the north gateway to the downtown.



- 18. Reconfigure Victoria Street and relocate the transit station.** It is recommended that the City explore the idea of relocating the transit station from the corner of Ward and Baker Streets to Victoria Street. Further consideration should also be given to the reconfiguration of the 100, 200, 300, and 400 blocks of Victoria Street into a one-way east configuration with angle parking on the north side of the street.
- 19. Convert parallel parking to angle parking in the downtown.** The conversion of parallel parking to angle parking in the downtown will increase street parking. One such exception is the 400 block of Baker Street where the parallel parking is retained in favour of “parking patios” during the summer.
- 20. Establish parking for electric vehicles and car share vehicles.** Specific locations throughout the downtown are recommended to support the use of electric vehicles and car share vehicles.
- 21. Introduce public washrooms.** Public washrooms are a necessary public service in the downtown. One washroom will be installed at the corner of Hall and Baker Streets. Other proposed locations for washrooms include, at the new transit station on Victoria Street and at the corner of Baker and Stanley Streets.
- 22. Add bicycle shelters.** Covered bike shelters will facilitate cycling in and around the downtown. A covered bike shelter is recommended at the new transit station on Victoria Street. Other recommended locations for bike shelters include the corner of Hall and Baker Street and the corner of Falls and Baker Street.
- 23. Support locally designed street furniture and art.** The bulb-outs at the street corners will be opportunities to support local artistic expression in public art and benches. Where possible, the design of these spaces should incorporate local art and furniture design to express the unique identity and culture of Nelson.
- 24. Support container plantings and hanging baskets.** It is recommended that the City continue to maintain the hanging baskets and planters that appear throughout the downtown. Container plantings adjacent to building entrances are recommended to provide individual expression to businesses. The containers should comply with the recommended standards for the individual streets.
- 25. Develop an “Adopt-a-Block” program.** The Adopt-a-Block program encourages businesses and property owners to coordinate improvements and seasonal enhancements on their block. This smaller-scale and more informal organization allows owners and businesses to more easily organize maintenance, seasonal plantings, or unique sidewalk or lane improvements on their blocks. This program should be supported by the City through technical coordination with other blocks in the downtown.
- 26. Create two incentive programs.** It is recommended that the City consider a signage and awning program that would provide matching grants to downtown businesses to install new signage and awnings, or to upgrade existing signage and awnings. Through the program business owners have access to technical assistance to help them design signage and awnings that are aligned with the City’s design standards.



4.3 IMPLEMENTATION TABLE

TABLE ABBREVIATIONS

NDCC	Nelson & District Chamber of Commerce
NAEDP	Nelson & Area Economic Development Partnership
CDC	Cultural Development Committee
NDAC	Nelson & District Arts Council

PHASE AND COMPONENT		DESCRIPTION	PROJECT LEAD	KEY PARTNERS	FUNDING	COST ¹
Short Term Priorities (1-2 years)						
1	Baker Street Stage 1	Reconstruct the 100, 200 and 300 blocks of Baker Street using the intermediate treatment for street improvements.	City	BC Transit, NDCC, CDC	Capital budget and provincial/federal grants	3 blocks x \$840,000/block = Est. \$2.52 million (intermediate treatment). A bike shelter in the 200 block is an additional \$30,000 to \$50,000.
2	Street Light Replacement	Replace street lights in two phases: Phase 1 – (Baker Street) 100 lights Phase 2 – (remaining streets in downtown) 232 lights, depending on condition. The recommendation is to increase lighting in the 400 and 500 blocks of Baker Street (east and west) pending verification from a lighting specialist.	City	MOTI	Capital budget	\$10,000 per light Phase 1 = Est. \$1.0 million Phase 2 = Est. \$2.32 million
3	Policy, Regulation & Guideline Amendments	Amend policy, regulatory and guideline documents to incorporate recommendations for signage, awnings and canopies, and outdoor patios.	City	NDCC, NAEDP, Land Owners	Development Services work program	Allocate \$25,000 consulting budget for rewriting policy and regulations
4	Gateway signage + Wayfinding	<ul style="list-style-type: none"> • Gateway sign at Baker Street at Hwy 3A entrance; • Gateway sign at Ward and Vernon Streets intersection; and • Secondary wayfinding markers at street corners; and • Digital sign pilot project. 	City	NDCC, NDAC, CDC NDCC (digital sign)	Capital budget	Baker Street at Hwy 3A: \$25,000 – \$50,000 Ward and Vernon Street intersection: \$25,000 - \$35,000 Secondary wayfinding markers: \$10,000 - \$20,000 each Digital Sign: Price TBD
5	Baker Street + Hwy 3A Intersection Improvements (refer to Railtown SNAP for more information)	Work with MOTI to increase pedestrian safety including painted crosswalks, removal of the right turn option, construction of corner bulb-outs, exploring the use of streetlights/round-about, and painting of parking stalls on the bridge.	City	MOTI, NDCC, local businesses	Capital budget	Crosswalk painting \$1,500; 4 corner bulb-outs \$65,000; remove right turn option \$15,000; install street lights \$150,000 (refer to Railtown SNAP) Est. \$231,500



Phase and Component		Description	Project Lead	Key Partners	Funding	Cost ¹
Short Term Priorities (1-2 years)						
6	Ward and Vernon Streets Intersection + Hwy 3A Intersection Improvements	Work with MOTI to increase pedestrian safety at intersections including painted crosswalks and construction of 2 corner bulb-outs, if right turn movements permit.	City	MOTI, NDCC, local businesses	Capital budget	Crosswalk painting: \$1,500; 2 corner bulb-outs: \$40,000 ² = est. \$41,500
7	Lanes Alive Program Stage 1	Improvements to the aesthetics and function of the lanes. Adding pedestrian and bike infrastructure. Focus upgrades to 400 and 500 blocks of the lanes, between Josephine and Stanley Streets.	City	CDC, NDAC, adjacent owners and businesses	Capital budget and local businesses	\$100,000 for lighting and painting x 2 blocks x 2 lanes = \$400,000 (est.)
8	Baker Street Adopt-A-Block Incentives	Implement a matching grants program to incentivize façade improvements including building lighting, signage, awnings and canopies, and outdoor patios.	City	NDCC, NAEDP, adjacent owners and businesses	Capital budget and provincial/federal grants	Recommended pilot program: potential 10 matching grants first year of \$5,000 to \$10,000 for a total \$50,000 to \$100,000 per year; total of \$150,000 to \$300,000 committed over 3 years.
Medium Term Priorities (3-5 years)						
9	Baker Street Stage 2	Reconstruct the 400 and 500 blocks of Baker Street, as well as Ward Street between Victoria and Vernon Streets using the premium treatment. The reconstruction includes a redesign of the public amenity areas, as well as the addition of new street and sidewalk treatments, catenary lighting at the intersection of Ward and Baker Streets, a community table and overhead trellis, street furnishings, and a water fountain.	City		Capital budget and provincial/federal grants	\$1.1 million per block x 4 blocks = est. \$4.4 million (premium treatment).
10	Victoria Street Stage 1	Redesign the 200 and 300 block to a one way road (eastbound) using the standard treatment. Covert existing parallel parking to angle parking, where possible. Redesign the 400 block using the standard treatment as well.	City	BC Transit	Capital budget and provincial/federal grants	\$660,000 per block x 3 blocks = est. \$1.98 million (standard treatment).
11	Relocate Bus Transit Stop to Victoria Street	400 block and facilities including bus shelter, bike shelter, public washroom and water fountain	City	BC Transit, adjacent land owners and businesses	Capital budget and provincial/federal grants	Bus transit stop = \$250,000 to \$350,000 Bus shelter = \$100,000 to \$150,000 Bike shelter = \$30,000 to \$50,000 Washroom = \$100,000 to \$130,000 Water fountain = \$6,000
12	Lanes Alive Program Stage 2	Improvements to the aesthetics and function of the lanes. Focus upgrades to 300 and 600 blocks of the lanes, between Hall and Kootenay Streets.	City, CDC	NDAC, adjacent landowners and businesses	Capital budget and businesses	\$100,000 for lighting and painting x 2 blocks x 2 lanes = est. \$400,000



Phase and Component		Description	Project Lead	Key Partners	Funding	Cost ¹
Long Term Priorities (6-15 years)						
13	Baker Street Stage 3	Reconstruct the 600 block using the intermediate treatment and the 700 and 800 blocks using the standard treatment.	City		Capital budget and provincial/federal grants	1 block x \$840,000 = est. \$840,000 (intermediate treatment) 2 blocks x \$660,000 = \$1.32 million (standard treatment) = est. \$2.16 million additional bike shelter cost in 700 block of \$30,000 to \$50,000
14	Victoria Street Stage 2	Reconstruct the 500 block using the standard treatment.	City		Capital budget and provincial/federal grants	Est. \$660,000 (standard treatment)
15	Vernon Street Stage 1	Reconstruct the 500 and 600 block (Ward Street to Hall Street), including sidewalks, median and parking. ²	City	MOTI	Capital budget and provincial/federal grants	2 blocks x \$840,000 = est. \$1.68 million (intermediate treatment) ²
16	Vernon Street Stage 2	Reconstruct the 700 and 800 blocks (Hall Street to Cedar Street), including sidewalks, median and parking.	City		Capital budget and provincial/federal grants	2 blocks x \$660,000 = est. \$1.32 million (standard treatment)
17	Lanes Alive Program Stage 3	Complete remaining blocks including the 100 and 200 blocks, and the 700 and 800 blocks in that order. Note: there is no lane in the 100 block of Herridge Lane.	City, CDC	NDAC, adjacent landowners and businesses	Capital budget and businesses	\$100,000 per block x 7 blocks = est. \$700,000
18	Other connector streets	Reconstruct the north/south connector streets starting at Ward Street and working east to west: Josephine Street (2 blocks, standard treatment) Stanley Street (2 blocks, standard treatment) Kootenay Street (2 blocks, standard treatment) Hall Street (already complete) Falls Street (1.5 blocks, standard treatment) Hendryx Street (1 block, standard treatment) Cedar Street (1 block, standard treatment)	City		Capital budget and provincial/federal grants	Remaining streets \$660,000 x 9.5 blocks = \$6.08 million (standard treatment)
Notes: 1. See section 4.4 for details on high level costing, including a description of “premium”, “intermediate”, and “standard” treatments. 2. Cost depends on design and treatment 3. Vernon Street Stage 1 – Assume that MOTI is responsible for improvements of the 100 to 400 blocks of Vernon Street, as it is Provincial highway.						



4.4 HIGH LEVEL COSTING

General Notes on High Level Costing

1. The cost estimates presented in this strategy are “high level” grade D costing based on preliminary estimates.
2. Cost estimates for street improvements do not include subsurface infrastructure improvements.
3. Further detail is needed on streetscape reconstruction requirements on a block by block basis. Costing may change based on the site specific conditions.
4. The cost estimates do not include a contingency, which could be up to 25% of the budgeted cost depending on selection of details and quality of products.
5. The structural soils for tree planting are included in the estimates. Silva Cells – type systems and associated special stormwater management infrastructure is not included in these estimates.
6. Construction costing regarding clearing of existing trees and street infrastructure is not included in the estimates.

The Three Cost Estimates for Street Improvements

There are three cost estimates that provide a variation of finishes. Each has a function that reinforces the pedestrian priority in the Downtown with the highest treatment in the core area.

- **Premium Cost Elements: \$1.1 million/block**

The “premium” treatment of street improvements is estimated at approximately \$1,100,000 per block. It includes street and sidewalk concrete and granite unit paving treatment, catenary overhead lighting at the Ward and Baker Street intersection, a custom-designed overhead trellis and community table in the large central public amenity area, benches, bike racks, street lighting, and a water fountain as well as more public art.

This “premium” treatment is reserved for the 400 and 500 blocks of Baker Street, and for Ward Street between Victoria Street and Vernon Street. These streets are in the highest pedestrian priority area.



- **Intermediate Cost Elements: \$840,000/block**

The “intermediate” treatment of street improvements is a similar to the “premium” treatment with a decreased level of detail and street furniture. For example, the level of concrete paving is decreased in the travel lanes, street furniture and design elements such as moveable chairs and tables are reduced, there is no overhead catenary lighting, there are half the benches, bike racks, trash receptacles and public art, there is standard street lighting, and there is no water fountain.

The “intermediate” treatment applies to the following:

- The 100, 200, 300 and 600 blocks of Baker Street.
- The 500 and 600 blocks of Vernon Street.

- **Standard Cost Elements: \$660,000/block**

The “standard” treatment of street improvement is intended to be a transition treatment to the adjoining residential neighbourhoods. No granite unit pavers are used in the sidewalks. Compared to the intermediate treatment half the benches, bike racks, and trash receptacles are used, no removable bollards and half the planters are applied. The result is still a Downtown distinction, but with less additional concentration of street furniture.

The “standard” treatment applies to the following:

- The 700 and 800 blocks of Baker Street.
- The 700 and 800 blocks of Vernon Street.
- The 200, 300, 400 and 500 blocks of Victoria Street.
- The connector streets including Josephine (2 blocks), Stanley (2 blocks), Kootenay (2 blocks) Falls (1.5 blocks), Hendryx (1 block) and Cedar (1 block).

Other Additional Cost Elements

- Bus Shelter: estimated at \$100,000 to \$150,000
- Bike Shelter: estimated at \$30,000 to \$50,000
- Public Washrooms: estimated at \$100,000 to \$150,000
- Water Fountains: estimated \$6,000 to \$10,000
- Electric Car-Charging Stations: TBD

Table 5: Comparison of Street Treatments

Streetscape Treatments	Premium	Intermediate	Standard
Vehicular Concrete Paving	Most (curb, parking and street)	Less (curb and parking separation)	Less (curb and parking separation)
Concrete Unit Paving	Granite Set paving	Granite Set paving	Concrete Unit Paving
Benches	32	16	8
Bike Racks	8	4	2
Bike Shelters		2	1
Removable Bollards	35	35	None
Street Lights	12	8	8
Catenary Lighting	At the intersection at Ward and Baker Streets – the “Town Square”	None	None
Movable Chairs	16	None	None
Movable Tables	4	None	None
Trellis and Community Table	Special	None	None
Trash Receptacle	8	4	2
Water Fountain	2	0	0
Public Art	4	2	None
Public Washroom			1
Transit Station			1
Planters	Yes	Yes	Yes



Implementation Street Treatment

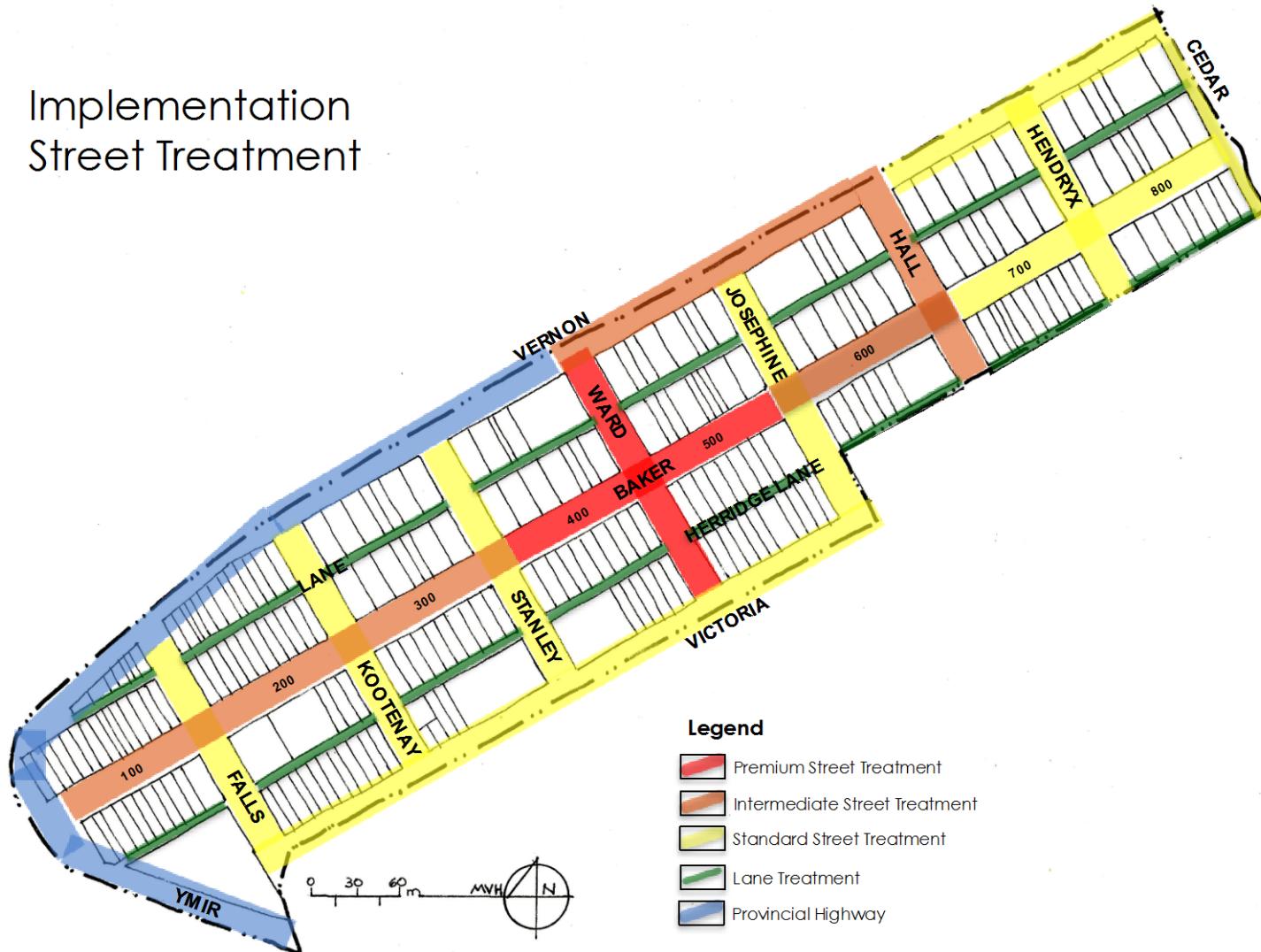


Figure 56: Implementation of street treatments



Table 6: Premium costing per block

City of Nelson
Nelson Urban Design Strategy
16-006

Perry + Associates Inc.
Consulting Landscape Architects
December, 2016

BAKER STREET PREMIUM COSTING PER BLOCK					
SCHEDULE OF QUANTITIES AND PRICES					
Item	Description of Work	Unit	Est. Qty	Unit Price	Total Price
A.	PAVING				
1.	Roadway				
1.1	Vehicular Concrete Unit Paving	sq.m	1,122	\$ 220.00	\$ 246,840.00
1.2	Barrier Curbs	Lin.m	244	\$ 160.00	\$ 39,040.00
1.3	Flush Concrete Banding	Lin.m	230	\$ 100.00	\$ 23,000.00
2.	Sidewalk				
2.1	Granite Set Unit Paving	sq.m	860	\$ 240.00	\$ 206,400.00
2.2	Sandblast Sawcut Concrete	sq.m	220	\$ 100.00	\$ 22,000.00
2.3	Stone Inset Feature Paving at Bulb outs	Each	8	\$ 500.00	\$ 4,000.00
	SUBTOTAL PAVING				\$ 541,280.00
B.	SITE FURNISHINGS				
3.	Furniture				
3.1	Benches	Each	32	\$ 2,200.00	\$ 70,400.00
3.2	Bike Racks	Each	8	\$ 1,500.00	\$ 12,000.00
3.3	Bollards	Each	24	\$ 1,500.00	\$ 36,000.00
3.4	Removable Bollards	Each	35	\$ 1,800.00	\$ 63,000.00
3.5	Street Lights	Each	12	\$ 10,000.00	\$ 120,000.00
3.6	Catenary Lighting c/w Extended Lamp Standard	L.S.	1	\$ 60,000.00	\$ 60,000.00
3.7	Movable Chairs	Each	16	\$ 400.00	\$ 6,400.00
3.8	Movable Tables	Each	4	\$ 1,200.00	\$ 4,800.00
3.9	Trellis / Special Amenity Area + Community Table	L.S.	1	\$ 40,000.00	\$ 40,000.00
3.10	Bear Proof Trash Receptacles	Each	8	\$ 2,500.00	\$ 20,000.00
3.11	Water Fountain w/ Plastic Water Bottle Filler	Each	1	\$ 6,000.00	\$ 6,000.00
3.12	Public Art	Each	4	\$ 10,000.00	\$ 40,000.00
3.13	Tree Grates	Each	14	\$ 1,000.00	\$ 14,000.00
	SUBTOTAL SITE FURNISHINGS				\$ 492,600.00
C.	PLANTING				
7.	Soft Landscape				
7.1	Street Tree Replacements, 9cm cal.	Each	14	\$ 750.00	\$ 10,500.00
7.2	Shrubs	sq.m	150	\$ 75.00	\$ 11,250.00
7.3	Perennials	sq.m	100	\$ 25.00	\$ 2,500.00
7.4	Annual Planting Beds	m ³	10	\$ 45.00	\$ 450.00
7.5	Planters	sq.m	80	\$ 200.00	\$ 16,000.00
7.6	Structural Soil per Tree	m ³	140	\$ 175.00	\$ 24,500.00
7.7	Root Barrier	Lin.m	42	\$ 20.00	\$ 840.00
7.8	Irrigation	Lin.m	200	\$ 20.00	\$ 4,000.00
	SUBTOTAL PLANTING				\$ 70,040.00
	TOTAL COST PER BLOCK (PREMIUM)				\$ 1,103,920.00



Table 7: Intermediate costing per block

City of Nelson
Nelson Urban Design Strategy
16-006

Perry + Associates Inc.
Consulting Landscape Architects
December, 2016

BAKER STREET INTERMEDIATE COSTING PER BLOCK					
SCHEDULE OF QUANTITIES AND PRICES					
Item	Description of Work	Unit	Est. Qty	Unit Price	Total Price
A.	PAVING				
1.	Roadway				
1.1	Vehicular Concrete Paving	sq.m	1,122	\$ 180.00	\$ 201,960.00
1.2	Barrier Curbs	Lin.m	244	\$ 160.00	\$ 39,040.00
1.3	Flush Concrete Banding	Lin.m	230	\$ 100.00	\$ 23,000.00
2.	Sidewalk				
2.1	Granite Set Unit Paving	sq.m	860	\$ 240.00	\$ 206,400.00
2.2	Sandblast Sawcut Concrete	sq.m	220	\$ 100.00	\$ 22,000.00
2.3	Stone Inset Feature Paving at Bulb outs	Each	8	\$ 500.00	\$ 4,000.00
	SUBTOTAL PAVING				\$ 496,400.00
B.	SITE FURNISHINGS				
3.	Furniture				
3.1	Benches	Each	16	\$ 2,200.00	\$ 35,200.00
3.2	Bike Racks	Each	4	\$ 1,500.00	\$ 6,000.00
3.3	Bollards	Each	24	\$ 1,500.00	\$ 36,000.00
3.4	Removable Bollards	Each	35	\$ 1,800.00	\$ 63,000.00
3.5	Street Lights	Each	8	\$ 10,000.00	\$ 80,000.00
3.6	Movable Chairs	Each	8	\$ 400.00	\$ 3,200.00
3.7	Movable Tables	Each	2	\$ 1,200.00	\$ 2,400.00
3.8	Bear proof Trash Receptacles	Each	4	\$ 2,500.00	\$ 10,000.00
3.9	Tree Grates	Each	14	\$ 1,000.00	\$ 14,000.00
3.10	Public Art	Each	2	\$ 10,000.00	\$ 20,000.00
	SUBTOTAL SITE FURNISHINGS				\$ 269,800.00
C.	PLANTING				
7.	Soft Landscape				
7.1	Street Tree Replacements, 9cm cal.	Each	14	\$ 750.00	\$ 10,500.00
7.2	Shrubs	sq.m	150	\$ 75.00	\$ 11,250.00
7.3	Perennials	sq.m	100	\$ 25.00	\$ 2,500.00
7.4	Annual Planting Beds	m ³	10	\$ 45.00	\$ 450.00
7.5	Planters	sq.m	80	\$ 200.00	\$ 16,000.00
7.6	Structural Soil per Tree	m ³	140	\$ 175.00	\$ 24,500.00
7.7	Root Barrier	Lin.m	42	\$ 20.00	\$ 840.00
7.8	Irrigation	Lin.m	200	\$ 20.00	\$ 4,000.00
	SUBTOTAL PLANTING				\$ 70,040.00
	TOTAL COST PER BLOCK (INTERMEDIATE)				\$ 836,240.00

Table 8: Standard costing per block

City of Nelson
Nelson Urban Design Strategy
16-006

Perry + Associates Inc.
Consulting Landscape Architects
December, 2016

BAKER STREET STANDARD COSTING PER BLOCK					
SCHEDULE OF QUANTITIES AND PRICES					
Item	Description of Work	Unit	Est. Qty	Unit Price	Total Price
A.	PAVING				
1.	Roadway				
1.1	Vehicular Concrete Paving	sq.m	1,122	\$ 180.00	\$ 201,960.00
1.2	Barrier Curbs	Lin.m	244	\$ 160.00	\$ 39,040.00
1.3	Flush Concrete Banding	Lin.m	230	\$ 100.00	\$ 23,000.00
2.	Sidewalk				
2.1	Concrete Unit Paving	sq.m	860	\$ 175.00	\$ 150,500.00
2.2	Sandblast Sawcut Concrete	sq.m	220	\$ 100.00	\$ 22,000.00
2.3	Stone Inset Feature Paving at Bulb outs	sq.m	8	\$ 500.00	\$ 4,000.00
	SUBTOTAL PAVING				\$ 440,500.00
B.	SITE FURNISHINGS				
3.	Furniture				
3.1	Benches	Each	8	\$ 2,200.00	\$ 17,600.00
3.2	Bike Racks	Each	2	\$ 1,500.00	\$ 3,000.00
3.3	Bollards	Each	24	\$ 1,500.00	\$ 36,000.00
3.4	Street Lights	Each	8	\$ 10,000.00	\$ 80,000.00
3.5	Bear Proof Trash Receptacles	Each	2	\$ 2,500.00	\$ 5,000.00
3.6	Tree Grates	Each	14	\$ 1,000.00	\$ 14,000.00
	SUBTOTAL SITE FURNISHINGS				\$ 155,600.00
C.	PLANTING				
7.	Soft Landscape				
7.1	Street Tree Replacements, 9cm cal.	Each	14	\$ 750.00	\$ 10,500.00
7.2	Shrubs	sq.m	150	\$ 75.00	\$ 11,250.00
7.3	Perennials	sq.m	100	\$ 25.00	\$ 2,500.00
7.4	Annual Planting Beds	m ³	10	\$ 45.00	\$ 450.00
7.5	Planters	sq.m	40	\$ 200.00	\$ 8,000.00
7.6	Structural Soil per Tree	m ³	140	\$ 175.00	\$ 24,500.00
7.7	Root Barrier	Lin.m	42	\$ 20.00	\$ 840.00
7.8	Irrigation	Lin.m	200	\$ 20.00	\$ 4,000.00
	SUBTOTAL PLANTING				\$ 62,040.00
	TOTAL COST PER BLOCK (STANDARD)				\$ 658,140.00



4.5 CONSTRUCTION METHOD

Street reconstruction and enhancements will require a coordinated approach among the City of Nelson, downtown businesses, and property owners. Concerns about the time of year and the duration of construction, as well as disruptions to access to the downtown are important to consider when planning for construction.

There are different potential methods to re-construct Baker Street and other streets in the downtown. Construction can take place block by block, one side at a time, or for the full corridor. In addition, reconstruction can take place during regular work hours, off-peak hours, or continuously during both periods to complete the work faster, but at a greater expense.

It is important to note that a combination of methods can work in one section of the downtown and not in another. Therefore, it is important that the methods employed and the timing of construction be specific to the area being considered for reconstruction. This may include consideration of adjoining land uses, traffic volumes and requirements for accessibility. Lessons learned from the Hall Street revitalization project should form the basis for planning for the Baker Street re-construction.

It is recommended that the City of Nelson work closely with the business community to minimize potential impacts. The following process is recommended when planning for construction:

- **Learn from Hall Street:** First, the City of Nelson should review the lessons learned from the Hall Street revitalization project and use that experience to develop a construction plan for Baker Street.
- **Create Efficiency and Minimize Disruption:** The construction method should be efficient and create the least disruption to adjoining landowners and businesses. A block by block construction method is recommended.
- **Develop a Communication and Public Engagement Plan:** A public engagement and communications plan is recommended for reconstruction projects to keep key stakeholders and greater community informed of updates, traffic rerouting, phasing, and schedules. It is recommended that the City use tools such as the website, social media, direct mail and face-to-face communications to keep interested parties informed.

